

West of England Office, Wilder House, Wilder Street, BS 2 8PH

Department for Transport
Great Minster House
33 Horseferry Road
London SW10 4DR

22 April 2013

Dear Sir or Madam,

West of England Cycle City Ambition Statement of Intent

The West of England authorities are committed to a vision of a safe and attractive on and off-road environment for cycling, building on the vibrant cycling culture that is developing throughout the area.

Our aim is to have 'more people cycling, more safely, more often' and to provide safe and accessible cycling for users aged from 8 to 80 years on all routes.

This requires a commitment to high quality routes with safe, convenient, efficient and attractive infrastructure with links between the main urban areas that have the potential to generate significant growth in levels of cycling. Traffic free routes and prioritisation of cyclists and pedestrians at crossings, cycle lanes, tracks alongside the carriageway and shared paths will provide a high quality environment for cycling.

Cycling in the West of England is already benefiting from the integrated approach of other transport initiatives; the introduction of 20 mph zones in Bath and Bristol helps to create a higher level of subjective safety and the existing resident parking zones in Bath and their planned further rollout in Bristol will encourage the greater use of sustainable transport for the shorter journeys.

Cycling infrastructure requirements are considered at the design stage in all developments and advice is provided to developers and development control as part of the usual way of working. This helps the development of a comprehensive network of cycle schemes amplifying the benefit of individual schemes.

Programmes to improve the levels of cycling via promotional activities, media campaigns, educational measures and signage take place regularly, linking in to the Public Health and the Community Safety responsibility of the councils.

We recognise the importance of links with partners in the voluntary sector, government, police, transport providers, landowners and business, health care, sports development and physical activity teams encourage the culture of cycling and promote it as the norm.



Bath & North East Somerset, Bristol, North Somerset and South Gloucestershire Councils
working together to improve your local transport

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The West of England has seen significant growth in cycling in recent years with the number of cycle journeys increasing by 31% between 2008 and 2012. For journeys to work we have the highest mode share in the South West and amongst the Core Cities.

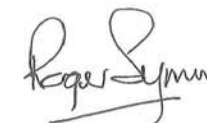
Cycle schemes recently completed include the Bristol to Nailsea cycle path, the Two Tunnels project along disused railway lines from Bath to Frome and the three routes between Parkway Station, Lockleaze and Cribbs Causeway retail centres.

Our firm, systematic and enthusiastic support for cycling will ensure that we build on this progress, and the Cycling City and Local Sustainable Transport Fund projects, and that more people cycle, more safely and more regularly. The Cycle City Ambition Fund will take us beyond our target to increase cycling by 78% (2008-2016) bringing benefits to health and wellbeing, reduction in carbon emissions and sustainable mobility across the West of England.

Signed,



Cllr Brian Allinson,
South Gloucestershire Council
Chair of the West of England
Joint Transport Executive Committee



Cllr Roger Symonds
Bath and North East Somerset
Council



Mayor George Ferguson
Bristol City Council



Cllr Elfan Ap Rees
North Somerset Council



Bath & North East Somerset, Bristol, North Somerset and South Gloucestershire Councils
working together to improve your local transport



Duncan Laird
 Group Manager
 City Transport
 3rd Floor Brunel House
 Bristol City Council

Reply to Dr Janet Maxwell
 Director of Public Health
 3rd Floor SLT Suite
Address City Hall
 College Green
 Bristol
 BS1 5TR
Telephone 0117 92 24570
Date 25th April 2013
Reference JM0004VK

Dear Duncan

Cycle City Ambition Fund bid

Public Health in Bristol has invested considerable resources towards transport planning over the past five years and is recognised nationally as a lead locality for advancing the transport and health collaborative agenda. Increasing cycling offers a significant and specific contribution to health. Thus, the opportunities afforded by the Cycle City Fund bid to make a major improvement to the cycle infrastructure in Bristol as well as within the sub-region remains a high priority for public health.

There is a clear need to build upon the success of our Cycling City work, maintain momentum, and so to ensure further growth in cycling. This growth, must, however, be shared across the population in order to achieve the greatest health improvement.

The Bristol cross-city Promenade project, as part of a 10-year vision for cycling, dovetails with the Local Sustainable Transport Fund work programme across and beyond the city. The Promenade project ensures that the investment will help maintain momentum and achieve the goal of more cycling shared across the population of Bristol.

In addition, the new structures for public health, which commenced on April 1st, enables the public health team to further advance collaboration with transport planning colleagues within the decision-making structures of the new health architecture. Part of this work includes ensuring that through our Joint Strategic Needs Assessment, aligning the work of the transport planning with that of the Health and Wellbeing Board is a priority.

Team/Service Area
 Public Health

Service Manager's Name
 Janet Maxwell
 Director of Public Health

Website
www.bristol.gov.uk

Increasing participation in active travel is an important way to help increase physical activity across all sections of the population and this funding opportunity will help us achieve this.

Yours sincerely

A handwritten signature in blue ink that reads "Janet Maxwell".

Dr Janet Maxwell

Bristol Director of Public Health

Email: Janet.Maxwell@bristol.gov.uk

Tel: 0117 900 2654

Team/Service Area
Public Health

Service Manager's Name
Janet Maxwell
Director of Public Health

Website
www.bristol.gov.uk



Department for Children, Adults and Health

Department for Transport
Great Minister Ho
33 Horseferry Road
London
SW1P 4DR

Date: 2nd April 2013
Your Ref:
Our Ref:
Enquiries to: Dr Mark Pietroni
Tel: 01454 864200
E-mail: mark.pietroni@southglos.gov.uk

2nd April 2013

To whom it may concern,

The cycling ambition grant provides a unique opportunity to promote and encourage cycling and walking and improve the health of the population in South Gloucestershire. We recognise the numerous health benefits that increases in physical activity can bring to individuals including a reduced risk of major illnesses, such as heart disease, stroke, diabetes and cancer. There are also a number of other public health benefits such as reductions in congestion, better air quality and improved road safety.

The aims of our bid align well with the priorities set out within South Gloucestershire's Draft Joint Health and Wellbeing Strategy. The strategy recognises that the greatest increases in physical activity will come from policies which improve the built environment and transport infrastructure through which active travel choices become the natural choices. Physical activity has been identified as a key priority outlining our ambition to support people to build everyday activity into their lives through walking and cycling.

The Health and Wellbeing Division endorse the cycling ambition grant bid and look forward to working in partnership with a number of agencies to help maximise the benefits that can ensue from a successful bid.

Yours sincerely

A handwritten signature in black ink, appearing to read "Dr Pietroni".

Dr Mark Pietroni
Interim Director of Public Health
South Gloucestershire Council

Health and Wellbeing Division, P.O.Box 2083, The Council Offices, Castle Street, Thornbury, Bristol, BS35 9BR



working together for health & well-being

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Your ref:
Our ref:
Date: 22nd April 2013

Adrian Clarke
Transportation Planning Manager
Riverside
Keynsham
BS31 1LA

Dear Adrian

The Seven Dials Cycling Scheme

I am pleased to support the Seven Dials Cycling Scheme. It will help to prioritise cycling and walking in the city centre and links to a number of other cycling initiatives across the city.

These include links to the Two Tunnels Project, the Bristol–Bath Railway Path and Kennett and Avon Cycle Route, the Colliers Way and the 5 Arches Connect2 cycle route. Links to the national cycle network are also currently under construction to the eastern and western parts of the city outskirts which should promote cycling and walking for work and leisure.

This will all be further enhanced by 20mph speed limits being extended to cover all residential areas of the district, including Bath city centre and the Seven Dials scheme, which are shown by evidence from elsewhere to encourage cycling and walking.

All of these plans, will help general connectivity by bike in and out of the city which will have positive effects on the health and wellbeing of residents and visitors for work or tourism. The Seven Dials Cycling Scheme acts as a central meeting point and focus for these paths and so is very important to get right to enhance connectivity across the whole network.

I am pleased to add my support for this scheme.

Yours sincerely

Paul Scott
Acting Director of Public Health
Bath & North East Somerset Council

Bristol Promenades Project

Proposals to enhance all of Bristol’s waterside quays, towpaths and walkways to create popular promenades to further promote walking and cycling in the area



April 2013



Bristol Promenades Project

Proposals to enhance all of Bristol’s waterside quays, towpaths and walkways to create popular promenades to further promote walking and cycling in the area

Introduction

Bristol’s riversides and docks have long been a popular place to cycle because they are attractive, central and flat. This project aims to draw them all together to make a real resource for the City, both for pedestrians and cyclists - all promenaders.

There are numerous problems to resolve, often matters which the public currently just cope with, but nonetheless are deterrents to easy and popular use. There are rough cobbled surfaces which are difficult to walk on, missing sections of route, busy roads to cross and sections which are of inadequate width.

This document lists the points which the current project is addressing in order to enable the City Council to have a coordinated and coherent framework for action over the coming years.

These notes include examples of the type of solution which the project is proposing. These are being revised and added to as the project progresses.

John Grimshaw
Coordinator of the
Bristol Promenades Project

Draft - April 2013



Vauxhall Bridge

Transforming Cycling in Central Bristol – Bridging the Water and connecting the Promenades

The waterways of central Bristol have come to be seen as the essence of the City. These are the focus of most of its visitor attractions, they are crossed by most journeys into the centre, they form the course of numerous traffic-free walking and cycling routes, and they are also a significant barrier for many journeys.

Any quest to increase walking and cycling in Bristol will include a Promenades project in its programme. The dockside quays and the riverside paths offer a sure way of creating attractive, continuous and popular routes free from traffic. Footbridges provide short cuts not available to motorists (There are 8 road crossings between Cumberland Basin and Temple Meads , and no less than 7 pedestrian bridges, most of which also cater for cyclists).

This report sets out to show how these waterside routes can be further enhanced, widened, extended and improved, all with the intention of popularising walking and cycling as a



particularly significant part of transport in the City, and a means of travel which is widely recognised as convenient, accessible, fit and healthy.

The report takes the form of numerous sketches, each one designed to address a particular issue which currently restricts the usage of the

waterside routes, or prevents any use at all. The lists of details is almost as long as you care to make it depending upon the standards and vision adopted. Each detail is valuable in its own right, and each could be brought forward as funds or parallel schemes allowed. Taken together the details amount to a strategy for putting the

waterside routes of Bristol at the heart of personal travel in the City, and can be seen as the focus and catalyst for city wide programmes aiming to double cycling levels again and again.

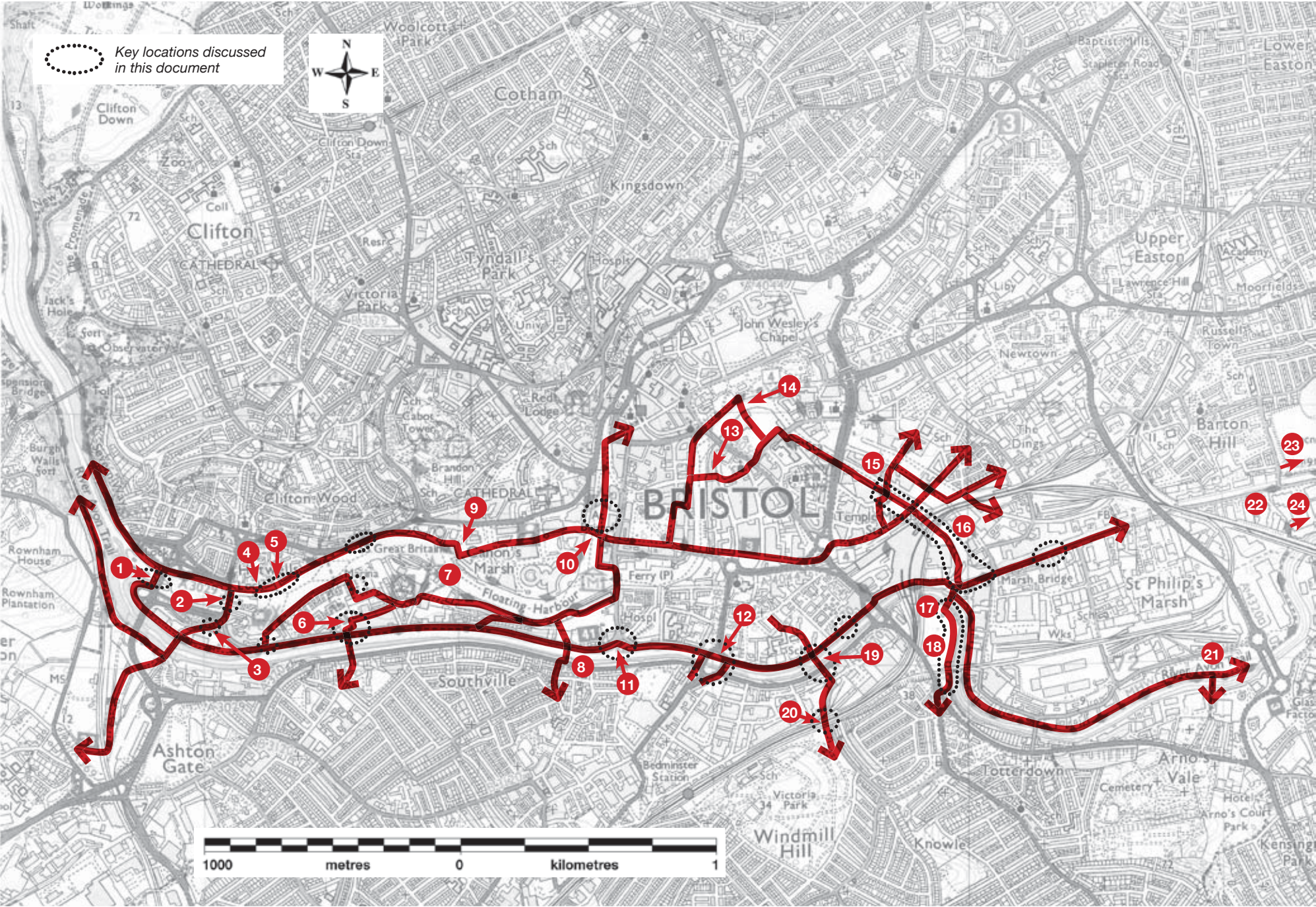
John Grimshaw
26.3.13



Transforming Cycling in Central Bristol – Bridging the Water and connecting the Promenades

A summary of the proposed programme of bridge works

- | | | | |
|--|---|--|---|
| <p>1 Add cycling way to top lock gates to cross Cumberland Basin.</p> <p>2 Widen footway across Cumerland Basin and extend width of footway across the old lock.</p> <p>3 Open up the former railway bridge under Smeaton Way to give a direct route from the riverside to the docks.</p> <p>4 Bridge across blocked up entrance to Merchant's Dock to overcome barriers if negotiation with landowners fails.</p> <p>5 Consider relocating Brunel's Swing Bridge to cross Poole's Dock entrance.</p> <p>6 Reconstruct north end of Vauxhall Swing Bridge to overcome ranks of steps and create direct route over river railway and road to Sydney Row which leads through to site of proposed Harbour Bridge.</p> <p>7 A Harbour Bridge just upstream of the SS Great Britain will need to be an iconic opening bridge or a chain ferry floating bridge with boat decks set at quayside level for convenient access and a bar/ café below overlooking the water.</p> <p>8 The Gaol Ferry Bridge will be linked direct to the harbour side by new development. Ensure that this is a quality route of sufficient capacity.</p> | <p>9 New Bridge across harbour inlet under construction as part of development.</p> <p>10 Pero's Bridge has already transformed walking and cycling in this area.</p> <p>11 Bathurst Basin Lock requires a new bridge to connect the riverside promenades. This need not open. Alternatively build up the concrete "dam" to road level.</p> <p>12 Bedminster Bridge is currently a major obstacle for cyclists both across the Cut and parallel to it as the gyratory does not have any provision for cyclists. The City's current design for completely remodelling this will give two excellent crossings of the river for cyclists and complete the north side promenade as well as connecting to an avenue up to St. Mary Redcliffe - <i>"the fairest, goodliest, and most famous parish church in England"</i>.</p> <p>13 The proposed King Street Bridge has been drawn up in detail and costed. It will provide a valuable direct connection across from Queen Square to Victoria Street and Old Market as well as linking to the riverside route under Temple Way to the station.</p> <p>14 The planned "Mobius" bridge is not ideal for cyclists in that it requires a lift to bypass the curved flight of steps at its start.</p> | <p>15 Existing 2 cycling bridges over the Floating Harbour.</p> <p>16 The planned Float Bridge under the railway bridges leading to Temple Meads will overcome a long standing obstacle to cycling in the area. It may be possible to open up historic vaults as an alternative.</p> <p>17 The planned Arena Bridge will have a wide cycling track which will lead to a promenade overlooking the river.</p> <p>18 A new bridge over the railway here will create a route to Three Lamps Junction and enable cyclists to bypass the extremely busy and wholly daunting Bath Road.</p> <p>19 The "Banana" Langton Street bridge is inaccessible to cyclists on account of steps either side. A scheme has been drawn up to replace these with ramped paving to make a direct route to the south of Bristol.</p> <p>20 A new "subway" driven level under the railway embankment to Victoria Park would radically open up this area because the dark and awkward St. Lukes Road Bridge is a real deterrent to cycling. A route then continuing around the edge of the park would form the basis of the direct cycling route to Hengrove with its colleges, hospital and community centres. Alternatively shift the road under</p> | <p>the bridge 1.4m to the east by removing the raised walkway that side and widen the more popular west side walkway by the corresponding amount.</p> <p>21 The Sparke Evans Park Suspension Bridge has no useful connection to the south. An immediate link could be made into the Paintworks employment area, and a continuous defined crossing of the Bath Road to Arnos Vale Cemetery would provide a daytime link to Totterdown.</p> <p>22 The planned link under Brunel's Bridge from Kings Park Avenue will link the Avon Meads Shopping Centre with the whole Barton Hill residential area.</p> <p>23 The planned Feeder Road Cycle Track along the whole length of its north side will connect to the New Brislington Bridge at Netham which has a good separated footway and will give the whole of St. Anne's access to the City Centre.</p> <p>24 To the east the planned bridge over the Avon at Hanham Mills will provide a direct, level and attractive route from the town of Keynsham to the centre of Bristol – 5 miles away.</p> |
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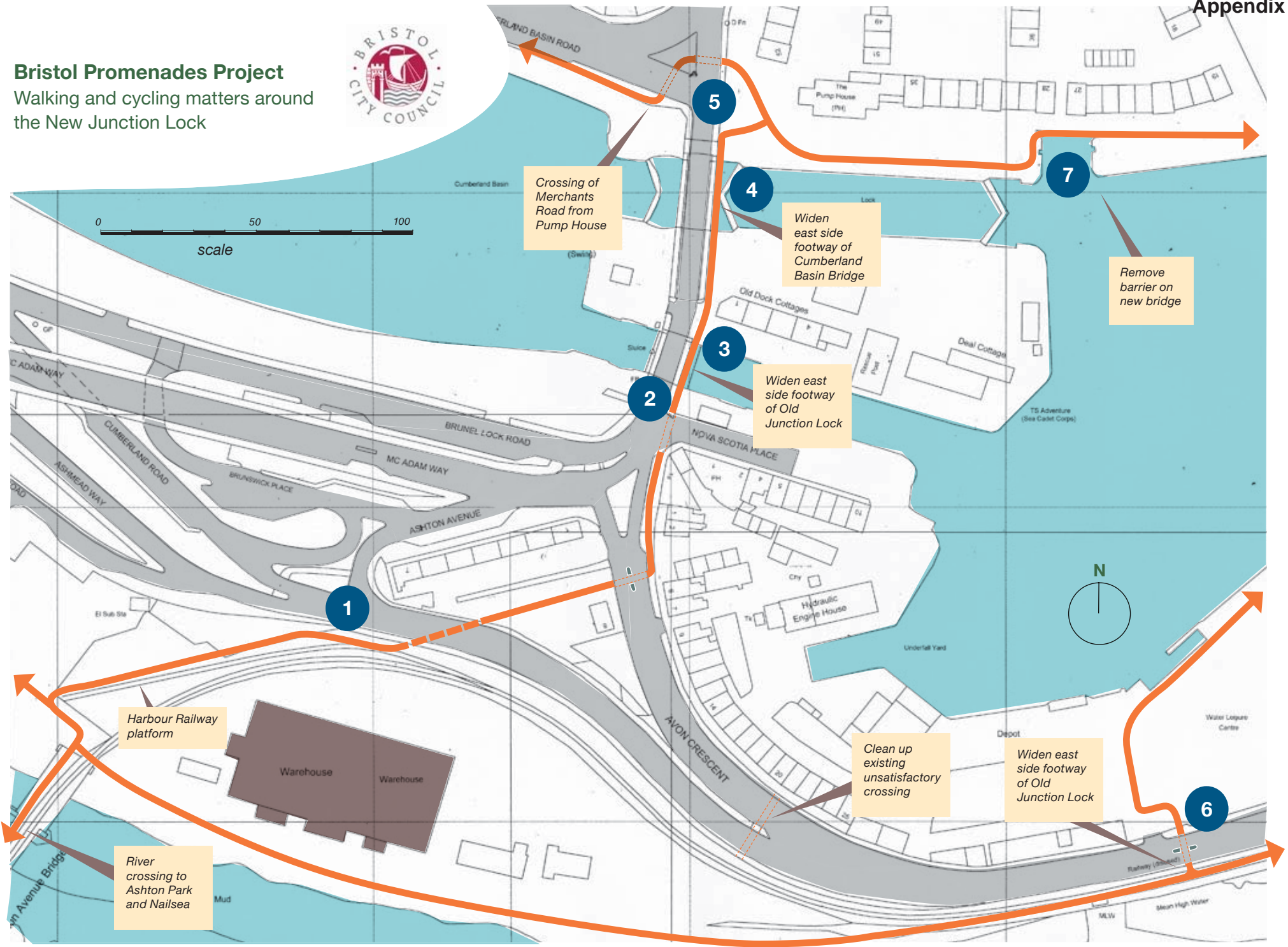
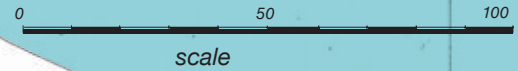


Bristol Promenades West: The Floating Harbour

- Detail** **Add developers plan**
1. Now that the new path has been constructed around the point of Brunel Lock Road, with its extensive views down the river, it is even more important to resolve the crossing of the lock gates and connections to Cumberland Basin Road, the paths to Hotwells and the bridge to Granby Hill. Despite opposition from English Heritage the existing walkway over the lock gates could be improved to cater for cyclist and wheelchairs and a new opening made in the railings to link direct to the existing cycling route along the road. The real advantages of a lock gate crossing is that it does not in any way add to the existing operational costs of managing the locking system.
 2. The existing cycling route beside the Cumberland Basin Road is all but inaccessible from the Pump House on account of the traffic here. The best solution will be a zebra crossing and revised road layout.
 3. The barrier here is frustrating and does nothing to aid safety. It should be modified or removed.
 4. The bridge over Poole's Dock entrance is rather unattractive in that it purports to be something it is not with only half swinging. The Promenades Project is an opportunity to "rescue" Brunel's Bridge from its oblivion under the Plimsoll Bridge and to relocate it in a prominent location, but one where it would not have the pressure and costs of regular and critical opening operations. If it was sited at Poole's Dock it would only have to open occasionally and simple mechanisms could be used as speed would not be a great consideration.
 5. The balustrade railings along Mardyke could be moved to nearer the dock side edge to give valuable additional width. Also new seating is required, set back against the road with acoustic backs so that the public can look out over this open stretch of water.
 6. Western Wharf has been finished off with irritating bands of protruding granite setts. These should be ground off flush to give a smooth passage but keeping the visual effect.
 7. The route past the current developments are provided for in a new bridge and connections to Gas Ferry Lane. For the time being the route will need to use the Lane until redevelopment on the west side is complete.
 8. The road crossing by the Silver Sphere is dangerous in that the car park vent shaft cuts off the sight lines. We need a consistent policy all through Brunel's Mile of raised zebra crossings similar to the excellent one across the road to the west.
 9. Pero's Bridge is too narrow for its current level of use but there is probably not much that can be done about this – the original design was correctly for a 5m width.
 10. The best route is through the Arena area possibly needs to be defined so as to reduce the likelihood of cyclist using the west side of St. Augustine's Reach which is not suitable for shared use on account of the restaurants and premises along the route.
 11. The problem of uneven setts has been nicely resolved along Narrow Quay by means of a smoothly laid insert to define the main walking route. Ideally this would be widened for cyclists as well. And a similar surface is needed around Arnolfini's Bush House.
 12. The current route over the west side of Prince Street Bridge peters out in this area of Prince Street. A well defined two way cycling route is needed as far as the Brunel Mile.
 13. At the south end of Prince Street Bridge another 2 way cycling route is required to reach the route along the south side of M Shed. This would discourage cyclists using the north side with its hazards of tram rails.
 14. The arrangement of the dockside route needs to be carefully detailed here with clear crossings of the rails so as to minimise accidents.
 15. The temporary route which recently opened was very welcome but care is needed to improve the proposed permanent route to Gaol Ferry Bridge.
 16. The road past the end of the sidings and through the car park needs to be defined and signed through to the Albion Dockyard.
 17. The Baltic Wharf revetments are barbaric in their deterrent to public use. Some simple way needs to be found for making this dockside a favoured sitting out area and we will show options for seating details down the slope to replace the existing precast concrete and stone units.
 18. The Chocolate Box Walk has a difficult surface for cyclist and spiked railings. It may be possible to gain some width because the railway corridor is not constant, and to relay the surface with more sympathetic but still "chocolate" box bricks, and to renew the railings. If the BRT2 scheme was to be revised to be a Tram route from Portishead then railings would not be required and further width could be enjoyed.
 19. A direct crossing over Cumberland Road to reach the Harbourside is required.
 20. The railway bridge under Cumberland Road is to be opened up under the BRT2 proposals to give a useful route to the Nova Scotia.
 21. For full benefit of the Cumberland Road Bridge link the existing walkway on the east side of the bridge should be widened as was proposed under the Connect 2 scheme.
 22. The Vauxhall Bridge stepped ramps at the northern end are a deterrent to cyclists and needed to be replaced as shown in the sketch.
 - A. The Riverside Path to Pill was constructed over 30 years ago and its renovation to a higher standard is overdue.
 - B. The Portway Path suffers from being adjacent to this main road.
 - C. The Festival Way to Bower Ashton, Long Ashton and Nailsea needs to be completed with the missing section across Ashton School Fields.
 - D. This new built link path is a welcome addition.
 - E. The Coronation Road riverside path could be so much more inviting than it is. The existing railings could be reset nearer the riverside, or removed altogether (?) with a fence nearer the water's edge so as to allow the use of a number of sections where there is the width for very small parks. At the very least space needs to be gained past each roadside tree, and new trees planted to complete this avenue.
 - F. Access to the riverside route is required at a number of points with zebra crossings of the main roads.
 - G. The shared path should be continued past Gaol Ferry Bridge as far as Bedminster Bridge because this would take some pressure off Gaol Ferry Bridge.

Bristol Promenades Project

Walking and cycling matters around the New Junction Lock



Crossing of Merchants Road from Pump House

Widen east side footway of Cumberland Basin Bridge

Remove barrier on new bridge

Widen east side footway of Old Junction Lock

Harbour Railway platform

River crossing to Ashton Park and Nailsea

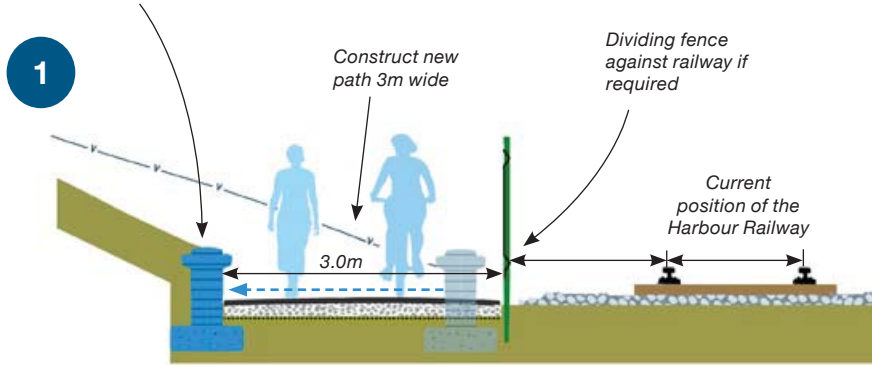
Clean up existing unsatisfactory crossing

Widen east side footway of Old Junction Lock

Bristol Promenades Project

Walking and cycling matters around the New Junction Lock

Reconstruct the dwarf retaining wall to new position and to new height, regrading the fill behind as much as possible as it was originally part of the old two decker approach to the Avenue Swing Bridge and so a good deal of the material is redundant now



Sketch showing approach to Cumberland Road Bridge with the Harbour Railway still in place



View approaching Cumberland Road Bridge showing route alongside railway

Modify the road layout to include a central island unless BRT works close off Avon Crescent to through traffic, which would be the ideal solution

2

Ample width for shared use

Raised pavement zebra crossing here



Existing gates across railway from Ashton Avenue Swing Bridge

View of link from end of railway to the Nova Scotia



John Grimshaw April 2013

Bristol Promenades Project

Walking and cycling matters around the New Junction Lock

Sketch of footpath extension required at Old Junction Lock

The deck can be supported on a series of steel beams cut into masonry wall on the one side, where they will gradually be above the line of services, and on the other by a steel beam running the length of the bridge. Alternatively have a separate bridge for cyclists set a little way out from the edge of the road.

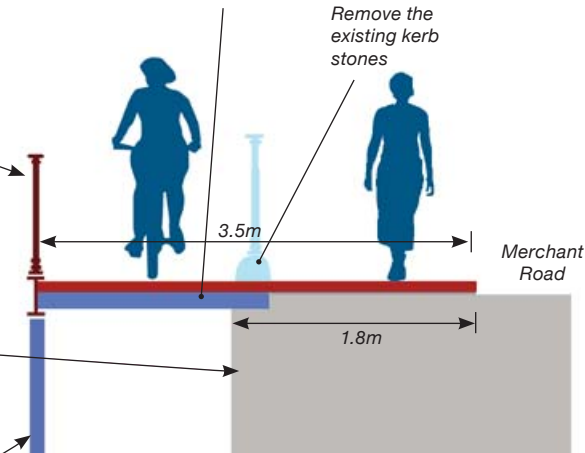
3

The existing balustrades could be revised or reproduced

Remove the existing kerb stones

The Lock is now dammed up with a masonry wall. This includes two sluices used to control water levels in the Decks. So any footway extension must be designed with a removable section should the cast iron sluice gate paddles need to be attended to or replaced

Steel columns can pick up the concrete walls below.

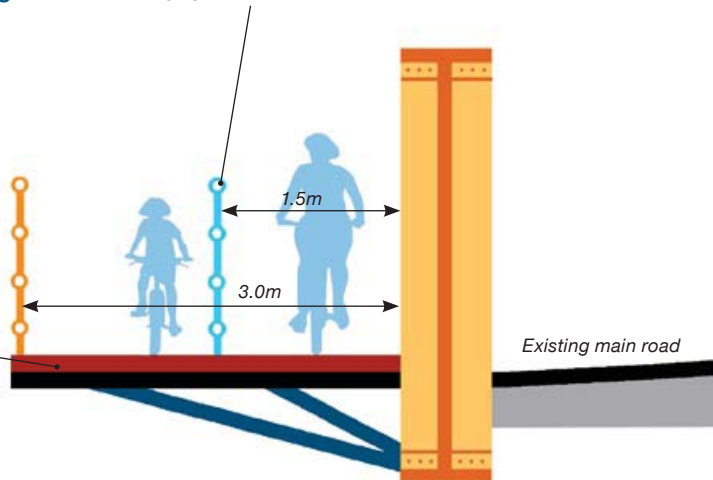


Sketch of widened walkway along east side of Cumberland Basin Bridge

4

Replace with new steel cantilever structure 3.0m wide, raising the balustrade railings if necessary. Note in the photograph that the lead in railings do not match and it would be best if the whole of this link over both junction locks was to have the same balustrade detail.

Existing footpath structure to be removed – the public can use the west side footway during these works



Footpath extension required at Old Junction Lock



Widened walkway along east side of Cumberland Basin Bridge



Bristol Promenades Project

Walking and cycling matters around the New Junction Lock

5

Beyond the Pump House entrance a stretch of wall could be usefully removed to make space for a much improved crossing of Merchants Road

Revise the island arrangement here so the public can cross these busy roads in two stages via zebra crossings

Construct a short length of new path around the end of the control house and take the railings around on a curve.



View of approach to Pump House entrance from the Cumberland Basin Bridge

Remove brick wall to show control house and to create space and visibility for turning into the Pump House area. Alternately construct a new steel ramp leaving the bridge immediately after its swing section to drop down to the Harbour side in front of the control cabin.

Recently constructed control house for lock gates and Swing Bridge

Make appropriate openings in the fences both sides of the railway. The roadside fencing is in disarray with sections repaired on an ad hoc basis with the dreadful palisade panels. Hopefully this can be put right should any BRT development take place on this route. Either use the "ghost" area to build a stand out for a single stage zebra crossing of the road, or rearrange traffic to accommodate a central island

Remove this brick panel to provide a good entrance to the Docks



View of desired crossing to Harbour side adjacent to Dock Office Entrance

Make formal crossing over the Harbour Railway 3m wide. Note this is to replace the current crossing to Avon Crescent so there will be no increase in railway operational costs or risk.

7

6

These railings take up more space than required and should be arranged to be closer to the water's edge

Existing barriers causes a great deal of frustration. They should be removed and a chicane placed across the side entrance as this carries far less people.

Ferry landing stage will be unaffected and the prow of the boats will be able to pass under the walkway extension

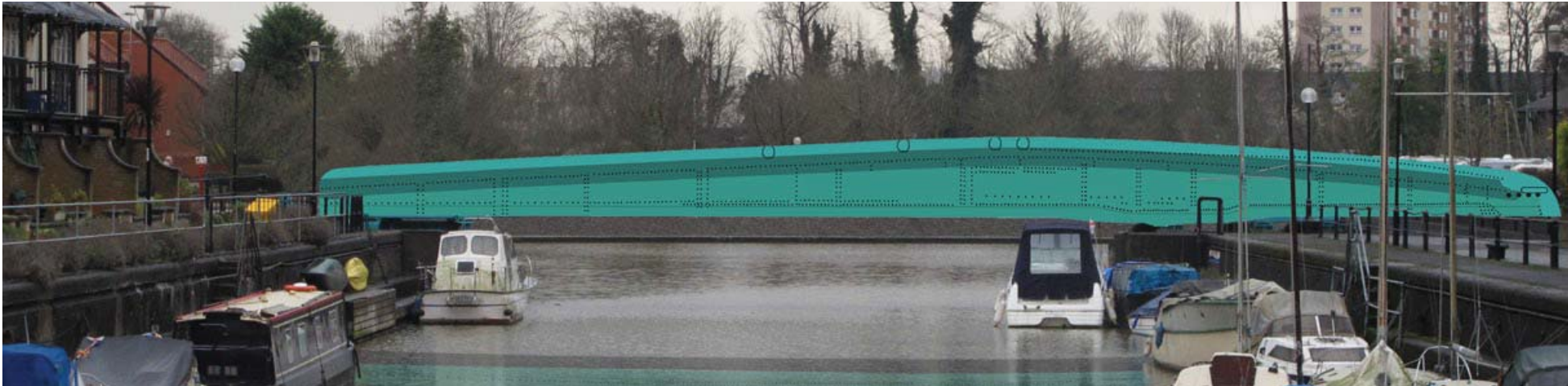
Extend existing walkway by a further 1.5 - 2.0m with a steel deck bolted onto the massive concrete beam cast across the sheet piling which closed off the deck.

New balustrades to match existing but with a third lower rail for young children



View of Cattle Market Dock entrance where the constructed walkway needs to be augmented

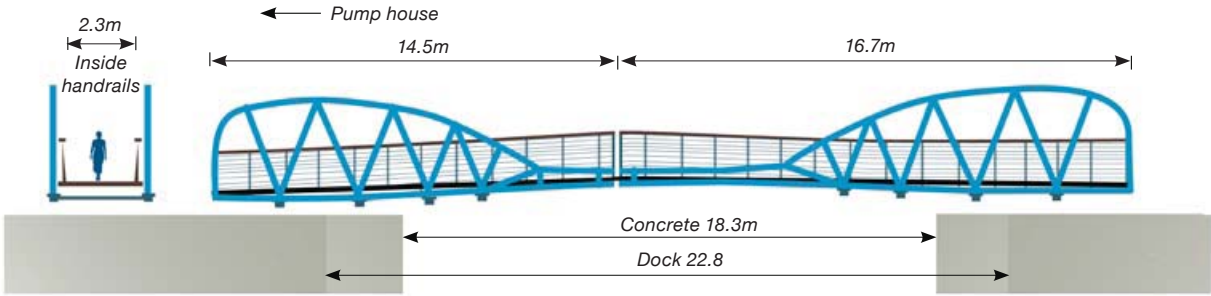




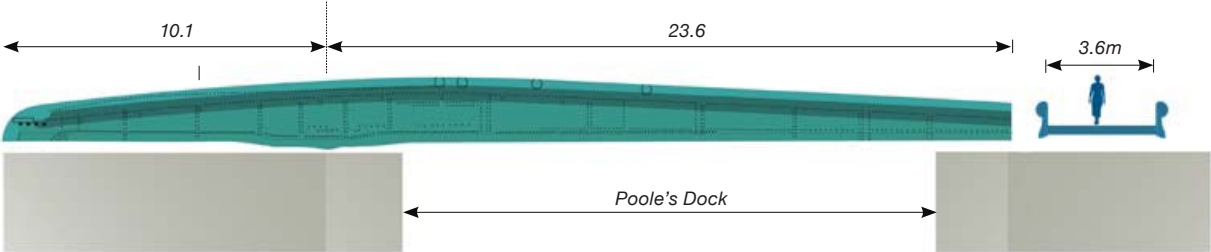
Bristol Promenades Project Brunel's Bridge for Poole's Dock

Brunel's historic tubular bridge - the prototype for a series ending with the Saltash Bridge over the Tamar - has languished for the past 40 years under the shadow of the Plimsoll Bridge at Cumberland Basin. The Bristol Promenades Project is an opportunity of recovering the bridge and giving it a new lease of life in full public view. Previous schemes have foundered on account of the cost of refurbishing it to the high standard which would be required if it was to operate across the lock entrance to Cumberland Basin. But here across Poole's Dock entrance it would only need to swing very occasionally, speed would be of no consequence, and gating arrangements could be simple and manual. The existing massive concrete entrance block would make a sound foundation for the bridge which could be fitted with a simple slim turning circle bolted direct to the concrete without the need for any alteration to the block. Turning could either be by a simple winch, or manually by means of a winding handle and cogged wheel. This modest work, combined with a new timber deck, could rescue this bridge to a position not far from its original site position and in sight of the Great Britain and the historic Underfall Yards.

The Promenades Project could then make good use of the existing turquoise bridges for necessary crossings elsewhere, including across the Bathurst Basin entrance.



Poole's Dock Showing the existing bridge



Brunel's Bridge viewed from the docks





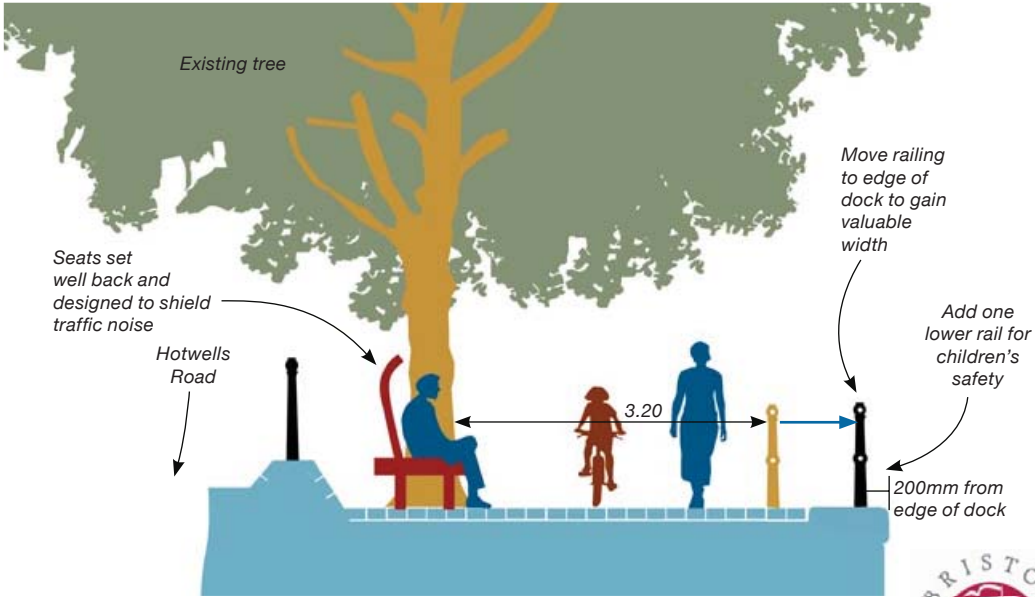
View showing railings moved to gain more space on promenade

Bristol Promenades Project
 Mardyke beside Hotwells Road – 200m

The Promenade along this section gives a particularly open view of the water and across to The Great Britain and boat yards. The adjacent main road is busy with traffic but its presence is softened by a line of mature trees. Two years ago the railings were moved a small amount to give more space over this section, but nearly a metre again could be gained if they were moved even closer to the edge of the dockside copings as shown in the sketch. This should not interfere with the workings of the Docks and moorings because at each iron bollard the existing railings break and this gap would be maintained.

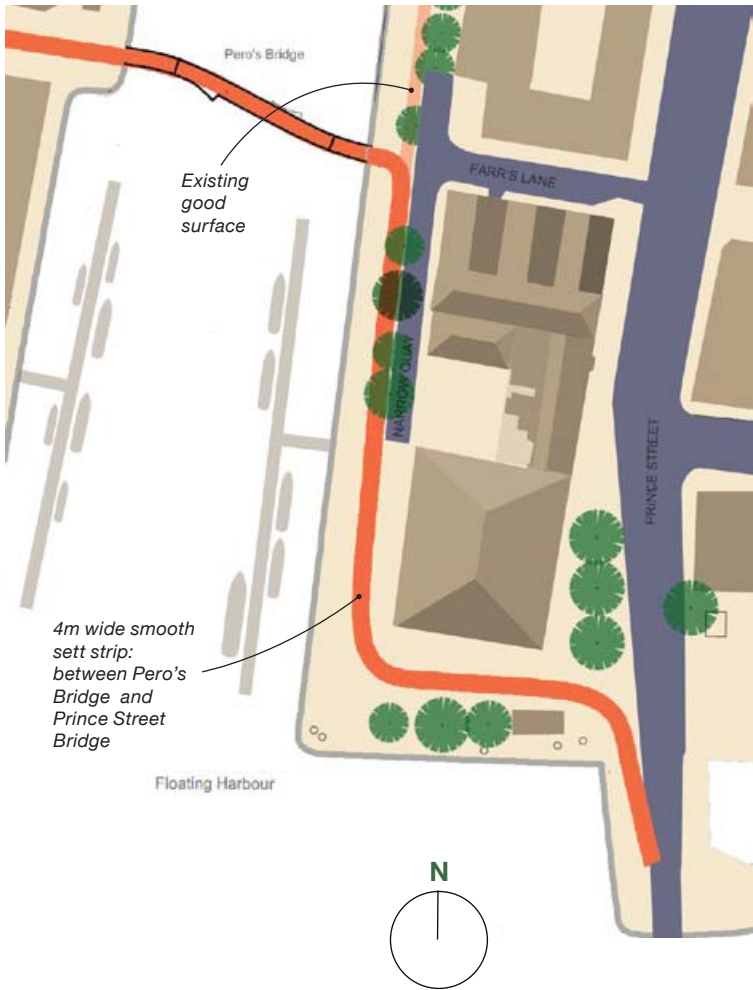
At the same time it might be worth adding a third, lower, rail to anticipate greater use of the Promenade including by families and children.

There were once some seats here but they were removed because they protruded into the Promenade. However there is space set back between the trees and a number of seats designed with acoustic backs to minimise traffic noise would make a very welcome stopping point to view the Docks and soak up the sun.



Sketch showing added width gained along Mardyke





Bristol Promenades Project
 St Augustine's Reach north of Pero's Bridge

Example of solution to uneven setts on historic quay sides. The inset path is carefully laid with fair faced stones to create an even and level surface which is much more suitable for general use, and cycling/wheelchairs in particular. Where possible this type of path should be laid 3m wide and this would be an appropriate solution for creating a continuous route around the south side of the Arnolfini to Prince Street Bridge.

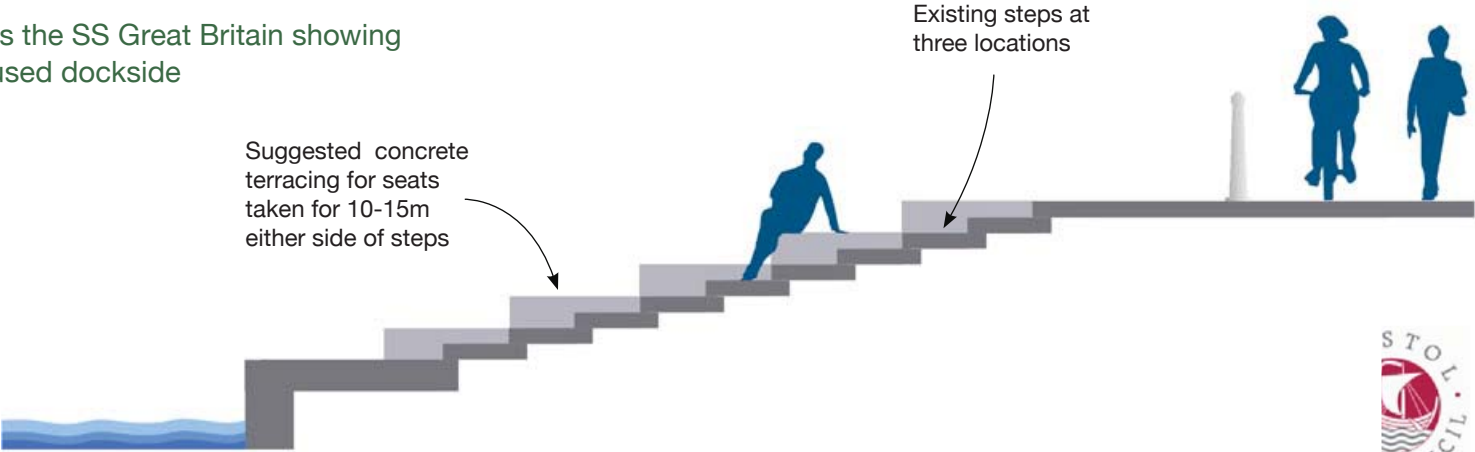


John Grimshaw February 2013



Bristol Promenades Project

View of Baltic Wharf looking towards the SS Great Britain showing terracing and seat on otherwise unused dockside



John Grimshaw February 2013

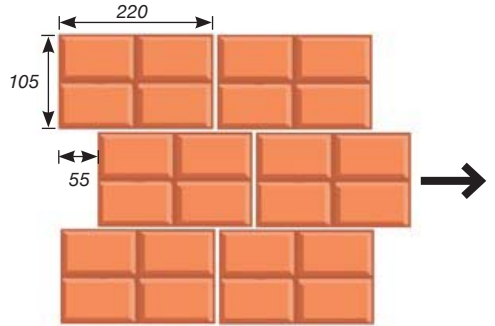




Bristol Promenades Project

The 'Chocolate' Walk looking east along the Cut showing a number of enhancements to this historic waterside route

1. Cut off the spikes from the existing railings and grind down flush with the top rail. This would greatly improve the apparent safety on this side. Where possible reset the fence to be 1.68m from the inside of the nearside rail. This gains a little extra width on some sections. Should the railway be converted into a tram route, then the fence can be disposed with altogether. In this case the dwarf wall at the foot of the railings could also be removed and flood protection achieved by raising the railway and the cycle track just beyond their bridge under Cumberland Road.
2. This pattern of blue engineering bricks is difficult to cycle on as narrow tyres can get 'tramlined'. The whole 'Chocolate' walk could be relaid (including sections to be repaired) with a somewhat different diagonal brick pattern. This would ride easily and also retain the effect. Alternatively stagger alternate rows.
3. This balustrade is fairly basic and could be replaced with a new one which was:
 - a. set out a little further with a cranked back post for a secure fixing.
 - b. curved to allow handlebars to fly over the foot of the post.
 - c. of a correct height for cycling - 1.4m.



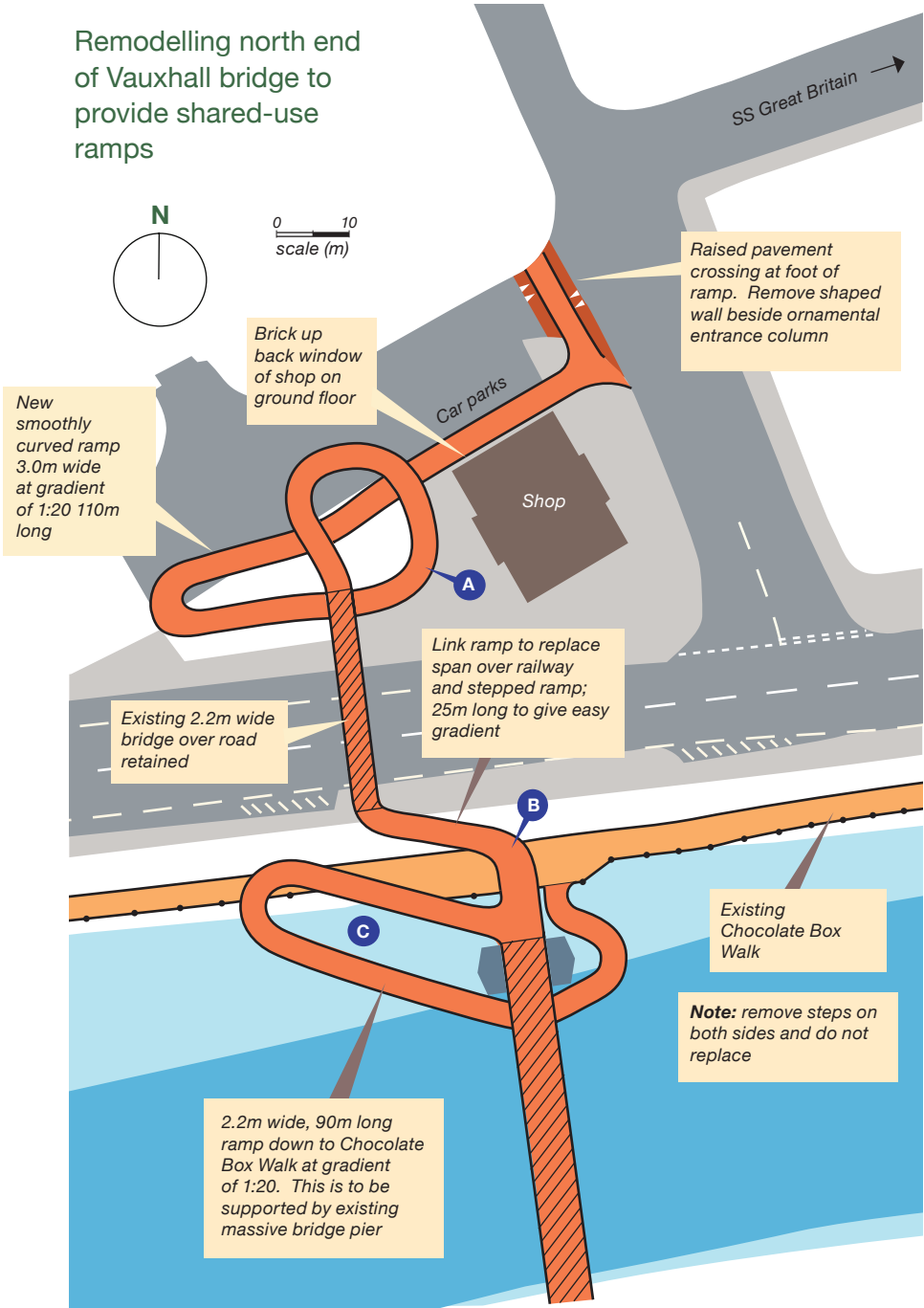
Option 1: Shift alternate rows



Option 2: Replace with diamond chequered paver



Remodelling north end of Vauxhall bridge to provide shared-use ramps



Bristol Promenades Project

Tackling the northern end of Vauxhall Bridge

Vauxhall Bridge has great potential. It is 3.5m wide and provides a route over the Cut which could provide the most direct route from North Street and The Tobacco Factory area to the City Centre via Greenway Bush Lane on the one side of the river and the waterfront on the other.

The only problems are the convoluted stepped ramps on the north side which greatly deter cyclists and almost eliminate prams and wheelchairs.

This sketch shows how to resolve this by some sinuous ramps which would also serve as a gateway feature to Bristol.

There are three components:

A is a 110m long ramp on the north side replacing the existing stepped ramps and steps. It will give a gradient no steeper than 1:20 and will emerge on the desire line to the harbour frontage and Sydney River.

B replaces the ungainly connecting stepped ramp at the south end of the road bridge. Here both the stepped ramp and the lattice bridge sections over the railway are removed so as to create a single ramp over 20m long dropping down at less than 1:20. Both this ramp and ramp A connect to the existing bridge over the road which although only 2.2m wide might as well be used as a secure component of the new bridge.

C is a much needed ramp down to the Chocolate Box walk replacing the existing steps. For support and as a feature it could wrap around the existing masonry pier for a secure foundation.

All these three ramps could be computer designed using the same lattice pattern as the existing swing bridge so as to give the impression of a single whole even though the ramps would be dipping and swooping. A pastiche of this lattice framework could even be attached to the sides of the remaining truss road section.



Bristol Promenades Central: Queen's Square and Temple Meads

Detail

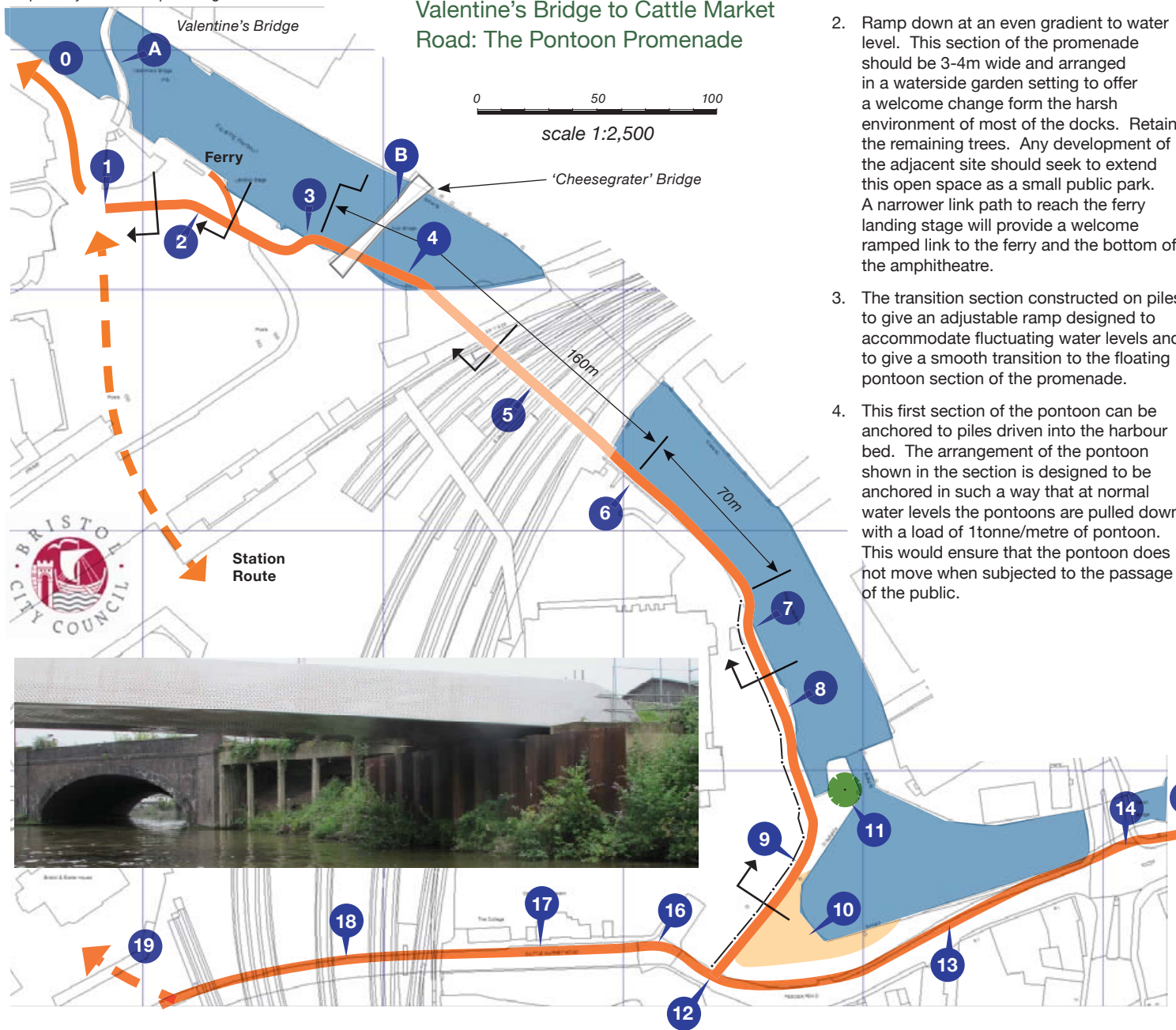
Add developers plan

1. The route from Queen's Square to Welsh Back needs defining.
2. Welsh Back's rough cobble surface needs a defined path in smooth stones over its whole length. The existing section opposite the Glass Boat needs to be doubled in width or the whole quayside re-laid.
3. The Bristol Bridge crossing needs additional signal time for the riverside route.
4. The planned "Moebius" bridge to the Brewery development has a considerable level difference to overcome and this has resulted in a pair of lifts on the brewery side. Whilst this is not ideal it still makes a valuable link in the route to Temple Meads.
5. A zebra crossing of Counterslip will be required to reach the existing riverside promenade.
6. This rubbish bin shelter obstruction needs to be removed or redesigned and the corner approaching the Temple Way Bridge will be eased by a short triangle of new promenade.
7. Existing "Bendy" Bridge is useful although the chicane barrier is frustrating.
8. New ramp to be incorporated within dockside development to take the promenade down to water level.
9. Floating promenade under the railway bridge through to the Post Office site is part of that redevelopment.
10. Promenade to be included in Post Office redevelopment on an interim basis if need be.
11. 3m wide shared use path along north side of Feeder will require a clear crossing of this junction.
12. The Arena Bridge is designed with separate wide pavements for shared use and a crossing of Cattlemarket Road to reach a 3.5m wide shared use path on its north side, and the Post Office Promenade.
13. The Arena site should include a route through to the Bath Road, ideally with a new bridge over the railway so as to emerge from the woodland direct at the Three Lamps junction.
14. Wheeling ramps at least would give access from the towpath route to Three Lamps and Totterdown.
15. A new link to Temple Meads is proposed in this redevelopment. Ideally this would continue all the way past, or under the Approach Ramp, to connect with Brunel's Mile.
16. The redesign of the Bath Bridge complex needs to provide for a direct and high quality route for pedestrians and cyclists without excessive delay. Some advantage can be taken of the semi circle of green to view the river and give some respite along the journey.
17. Widen the footway to give 6m all through (by removing parking or as part of the planned Clarence Road gyratory route). Great care is needed to progress towards a future scheme where the plane trees end up adjacent to the kerbside to give a 4m wide clear promenade. This can be achieved by replanting recent new trees and planning to replace older ones as they mature.
18. The steps up to the "Banana" bridge needs to be replaced with ramps.
19. Making space under the railway at St Luke's Road would give an invaluable route to the South.
20. Continue to achieve a 6m total width and make a zebra crossing to Ship Lane.
21. The promenade needs to continue across the Bedminster Bridge roundabout with a clearly defined route and generous timings for pedestrians and cyclist.
22. The link to Bedminster Parade needs to be formalised.
23. There are plans for a Redcliffe Hill Promenade which would be very welcome.
24. This is one of the best sections of the existing promenade with a wide path and mature trees. A zebra crossing to Bathurst Basin and the Ostrich Pub is required and the missing section approaching the Marchioness Building built.
25. Construct new promenade across missing gap.
26. A new bridge over the lock entrance is needed to give continuity, (as the footpath attached to the bridge is on its east side).

The Marchioness Building itself could have a public function which would be very welcome on account of its prominent location looking down the Cut, and "God's" Garden could be incorporated as a pleasant public area. It would also be possible to link through under the Commercial Road to reach the Bathurst Basin Dockside which would make a welcome link.
27. The route from the end of the existing bridge to Guinea Street is obstructed by tables and needs to be defined.
28. This potentially useful link around to Redcliffe Bridge, and a fascinating section of quayside in its own right, is all but impassable on account of the rough cobbles. Again this needs the solution of an inset strip of smoother stone.

Existing promenade passes under Temple Way to St. Phillip's Bridge

Bristol Promenades Project Valentine's Bridge to Cattle Market Road: The Pontoon Promenade



1. Start of new link at the top of the Amphitheatre
2. Ramp down at an even gradient to water level. This section of the promenade should be 3-4m wide and arranged in a waterside garden setting to offer a welcome change from the harsh environment of most of the docks. Retain the remaining trees. Any development of the adjacent site should seek to extend this open space as a small public park. A narrower link path to reach the ferry landing stage will provide a welcome ramped link to the ferry and the bottom of the amphitheatre.
3. The transition section constructed on piles to give an adjustable ramp designed to accommodate fluctuating water levels and to give a smooth transition to the floating pontoon section of the promenade.
4. This first section of the pontoon can be anchored to piles driven into the harbour bed. The arrangement of the pontoon shown in the section is designed to be anchored in such a way that at normal water levels the pontoons are pulled down with a load of 1tonne/metre of pontoon. This would ensure that the pontoon does not move when subjected to the passage of the public.
5. Under the railway vaults the pontoon will need to be anchored with concrete blocks, one 4m x 1.5m x 1.5m 20 tonne block every 5 metres. This arrangement will ensure that if the Dock levels rise, the pontoons will remain at the same level, even if they submerge a little and flood over the deck. This will keep them clear of the railway bridge structure at all times.
6. When the Post Office development is completed the pontoon route should connect to the scheme's waterside promenade at the first opportunity. This could happen now if the car park deck is to be retained in the planned scheme.
7. On an interim basis the pontoon will go a further 70m, again anchored by piles over this section, to reach a transition piece to rejoin the land. Promenade to be built to its final standard, but fenced off from the development site at this stage.
8. Review the security fencing to see if it could be made a more attractive feature.
9. The SSSI area is an attractive feature along the route.
10. This prominent tree could be featured with a circular seat.
11. Join the Cattle Market Road cycle route and the access crossing to the Arena site.
12. Widen this footway into the road to give a 3 – 3.5m wide shared use route.
13. Define crossings across junctions and allow for a short cycle crossing within the traffic lights sequence.
14. 3 – 3.5m wide shared use segregated path for the whole length of the Feeder.
15. Widen footway to 3.5m all through as part of the Arena bridge scheme.
16. Install artist based lighting installation to enhance this rather grim passage under the railway.
17. A new direct route to Temple Meads station to be incorporated in the redevelopment.

John Grimshaw February 2013

Existing temporary car park on site to be eventually redeveloped



Section near the start behind the amphitheatre

Remains of goods yard retaining wall which if it is to remain could be covered with creeper

Amphitheatre's back wall and seating dropping down to the ferry landing

Promenade path at least 3m wide dropping down at a gradient no steeper than 1:20

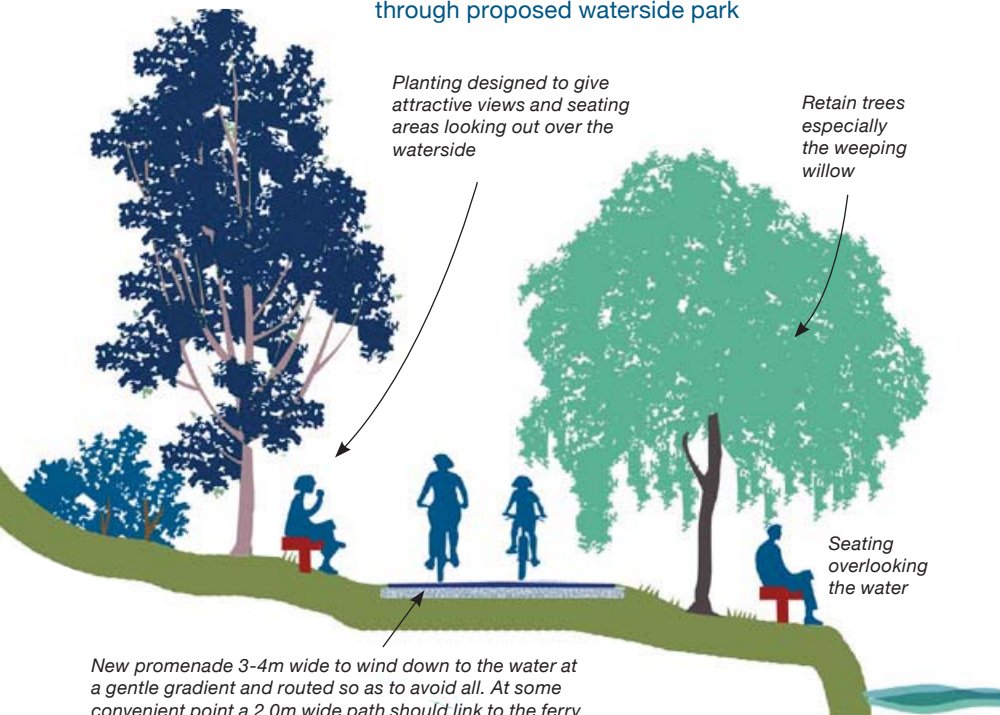
Temporary car park



Section dropping down to water level through proposed waterside park

Planting designed to give attractive views and seating areas looking out over the waterside

Retain trees especially the weeping willow



Seating overlooking the water

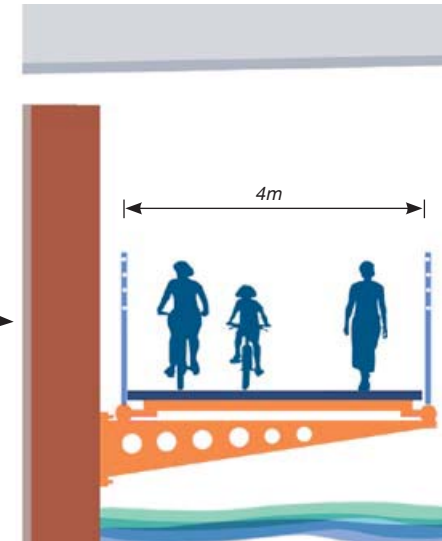
New promenade 3-4m wide to wind down to the water at a gentle gradient and routed so as to avoid all. At some convenient point a 2.0m wide path should link to the ferry landing

Bristol Promenades Project

Valentine's Bridge to Cattle Market Road



Existing sheet piling under 'Cheese Grater' bridge could be used as the connecting section to link to the floating pontoon section

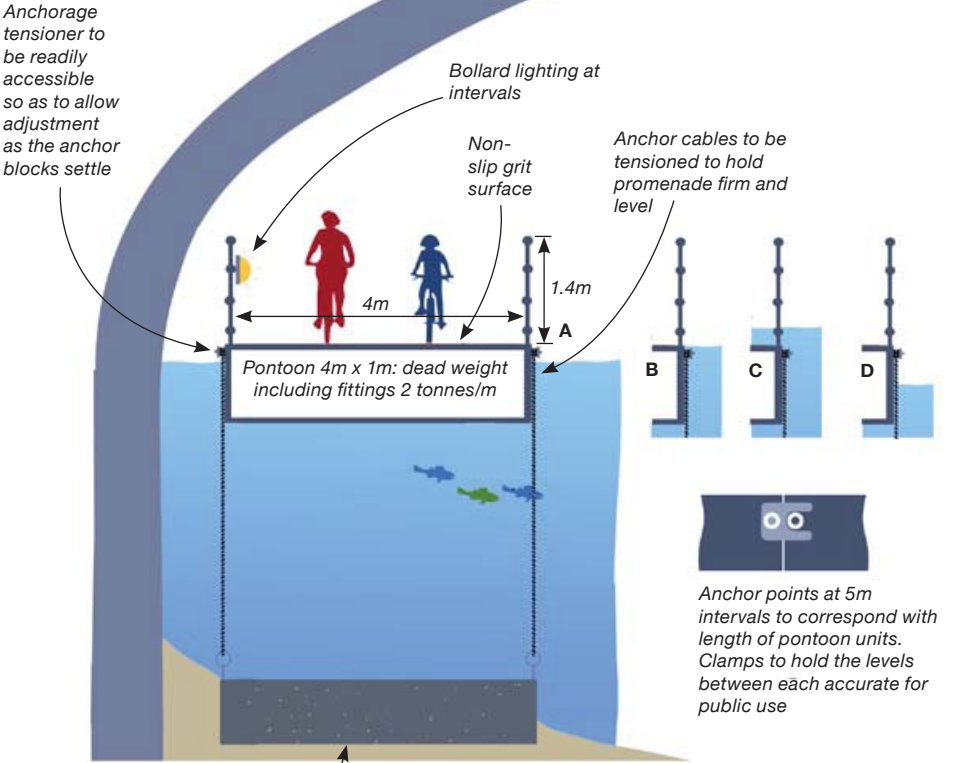


Bristol Promenades Project
 Valentine's Bridge to Cattle Market Road
 Floating units beneath railway



Views of river beneath railway

Cross section through floating units



- A.** Normal float 250, anchor load 1 tonne/m run
- B.** High water 00 freeboard, anchor load 2 tonnes/m
- C.** Flooded over promenade, anchor load 2 tonnes/m
- D.** Lowered water 500, anchor load zero and at lower levels the anchor cables go slack and pontoons can drift a little

Concrete anchor blocks fitted into silt on bed of harbour
 4m x 1.5m x 1.5m blocks at 5m spacing

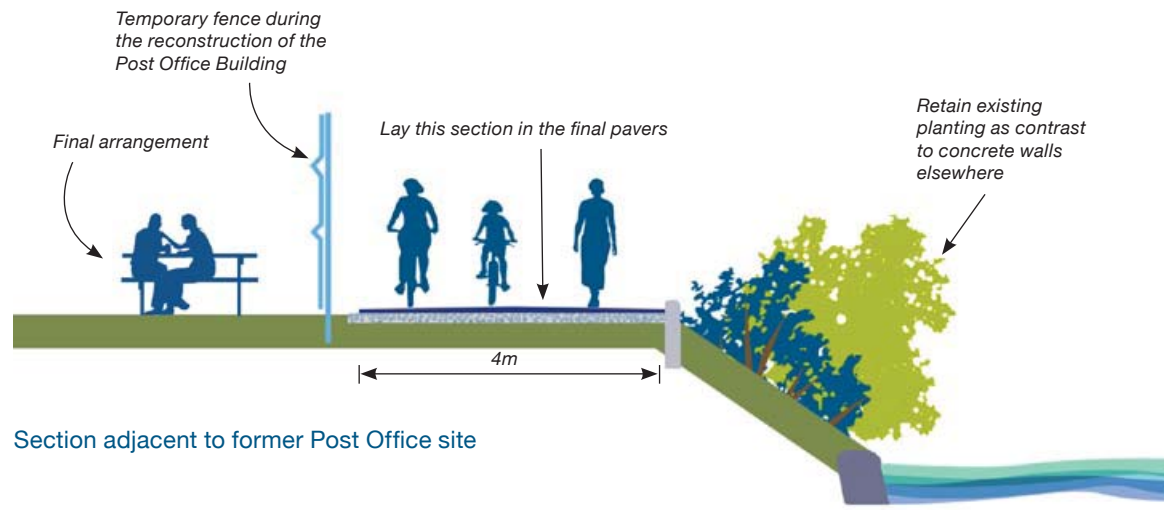
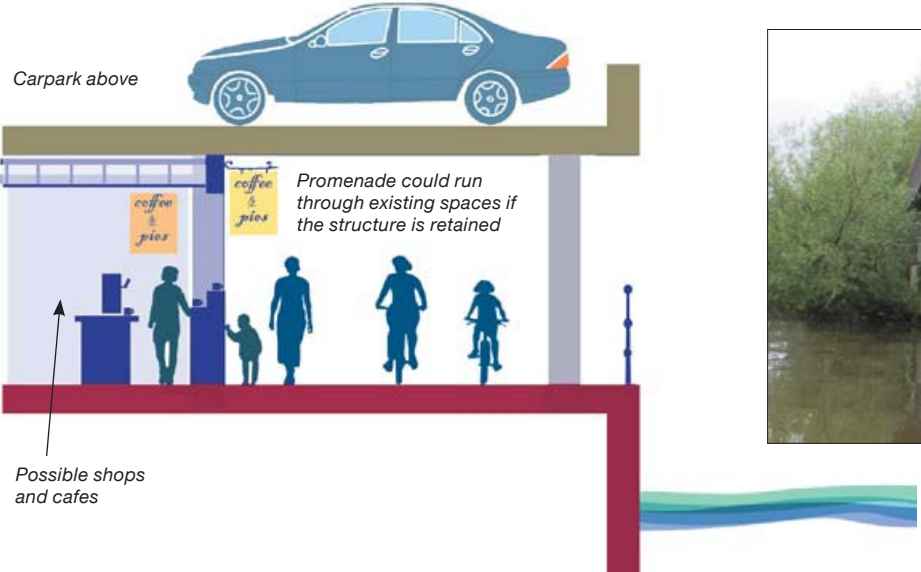
Options:
 Ground anchor?
 Dense aggregate?
 Allow for some silt anchorage effect?



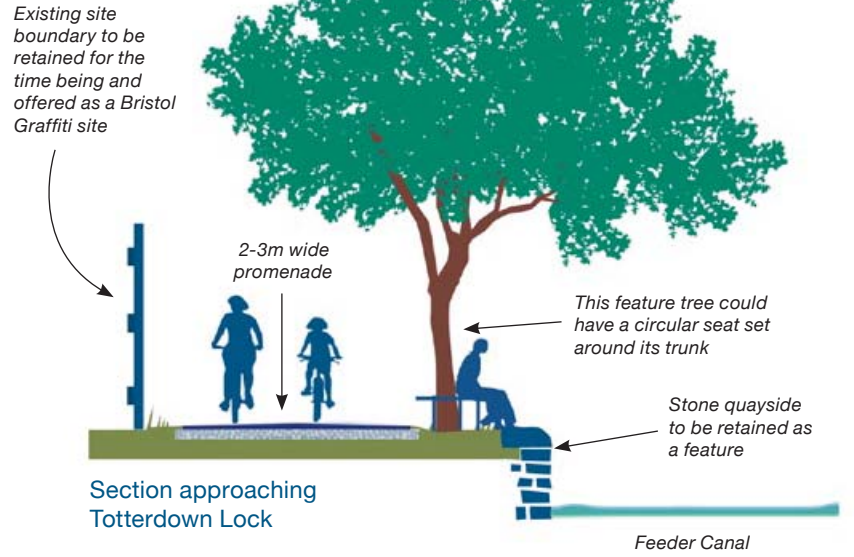
Bristol Promenades Project

Valentine's Bridge to Cattle Market Road

Past Post Office Structure



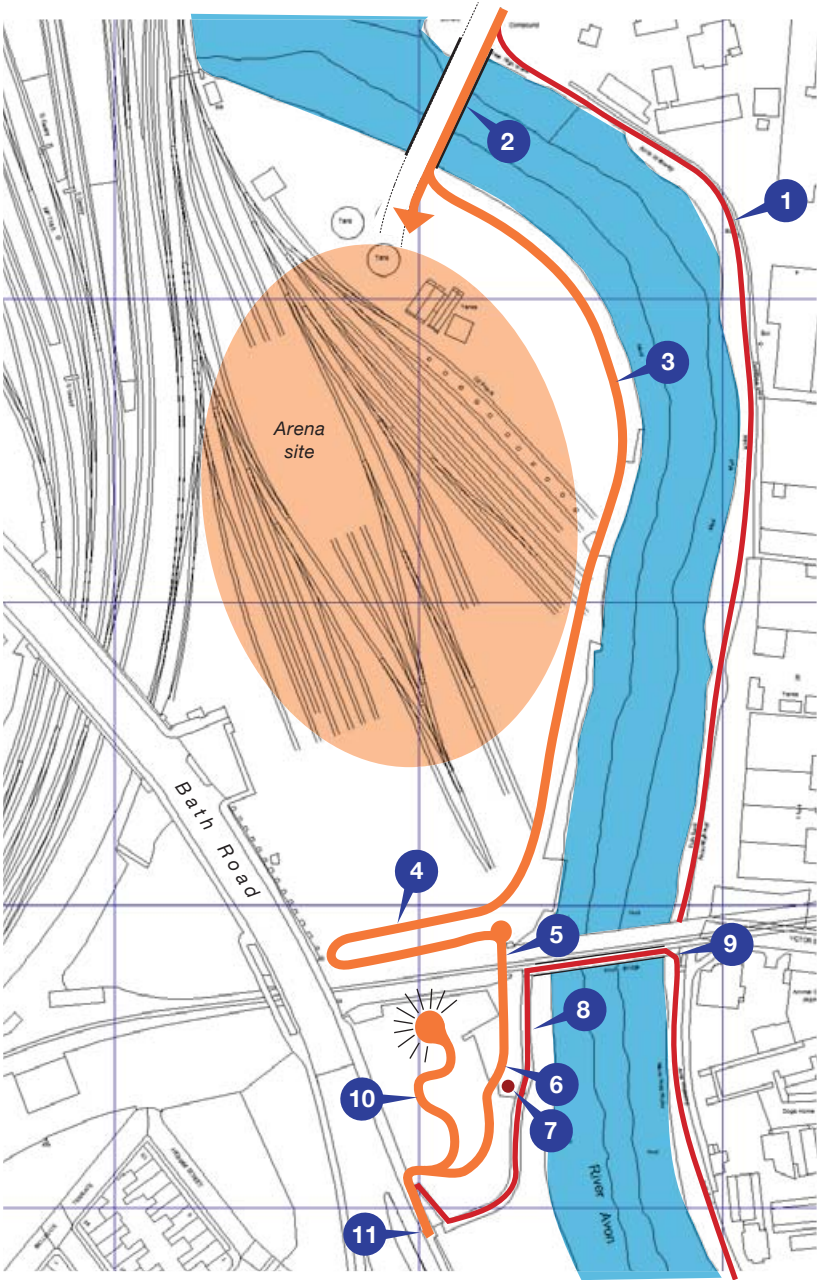
Section adjacent to former Post Office site



Section approaching Totterdown Lock

Bristol Promenades Project

Arena link to Three Lamps for Totterdown

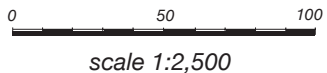


The redevelopment of the Arena site offers the chance of creating a promenade route overlooking the River from a vantage point perched high on the remains of old retaining walls and other railway structures. Such a promenade is likely to form the public face of the Arena, an informal public space and probably the means of access to the Arena area itself.

But also it has the potential to create a through route to the Three Lamps Junction which would then make for a way avoiding the A4 Bath Road for residents of Totterdown and Knowle you wish to walk or cycle through to Temple meads and the Broadmead area. This would be an immensely valuable addition to mobility in this area where the heavy traffic on the A4 represents a formidable barrier.

1. Existing Riverside Path. This will connect across Cattle Market Road to the floating Promenade to Temple Meads. This path is a possible route to Totterdown for pedestrians via the railway bridge and steps at Victoria Street (9) but it is not capable of development as a popular through route as the steps would be very difficult to modify.
2. Wide path to be incorporated in the Arena access bridge.
3. Promenade around Arena Site is likely to be wide enough to service the various entrances into the Arena as well as be served by refreshments and other services.

4. A zigzag ramp is needed to climb 6m to cross over the railway. This should be no steeper than 1:20. Again this maybe incorporated into the Arena access, so its form and location is only schematic here. Minimum width 3.5m should allow for parapets and walls.
5. Bridge over the railway could span onto a convenient point of the massive retaining wall to the south of the line. This will result in a very short span.
6. The path should be routed up through this abandoned site at an even gradient to reach the Three Lamps crossing of the A4.
7. At some convenient point make a connection to the lower path coming up from the railway bridge.
8. This is a beautiful path which deserves to be better known. It is an object lesson in making an organic line following the dictates of the steeply sloping ground.
9. These metal steps would be difficult to upgrade with a full ramp but the addition of wheeling ramps would be useful.
10. Make a winding path to a high level look out over the whole of the Arena site. This abandoned woodland could be managed as a woodland park and open space convenient for the Arena site.
11. Connect to path coming up from river and to make a good crossing of the A4 at Three Lamps.



Bristol Promenades Project

View of Feeder Road showing proposed arrangement of 3-3.5m wide shared use path to replace current footway and advisory cycle lane

All cars parked on south side of Feeder Road to be removed and edge of road painted with double yellow lines

Solid white line 100mm wide set 150mm from kerb edge to mark out edge of shared use path and give a feeling of security

Existing kerbs to be dug out and reset on the new road line approximately on the line of the advisory cycle lane



Path relaid on a smooth surface full width with central camber of 40mm

Drainage slots 100mm wide cut through copings at 10m centres to allow water on path to drain to grass below on side of canal

Granite setts recovered to store if required or otherwise buried by new extended pavement

Existing drainage gullies to be repositioned on the edge of the new road line and connected back to the existing drainage system

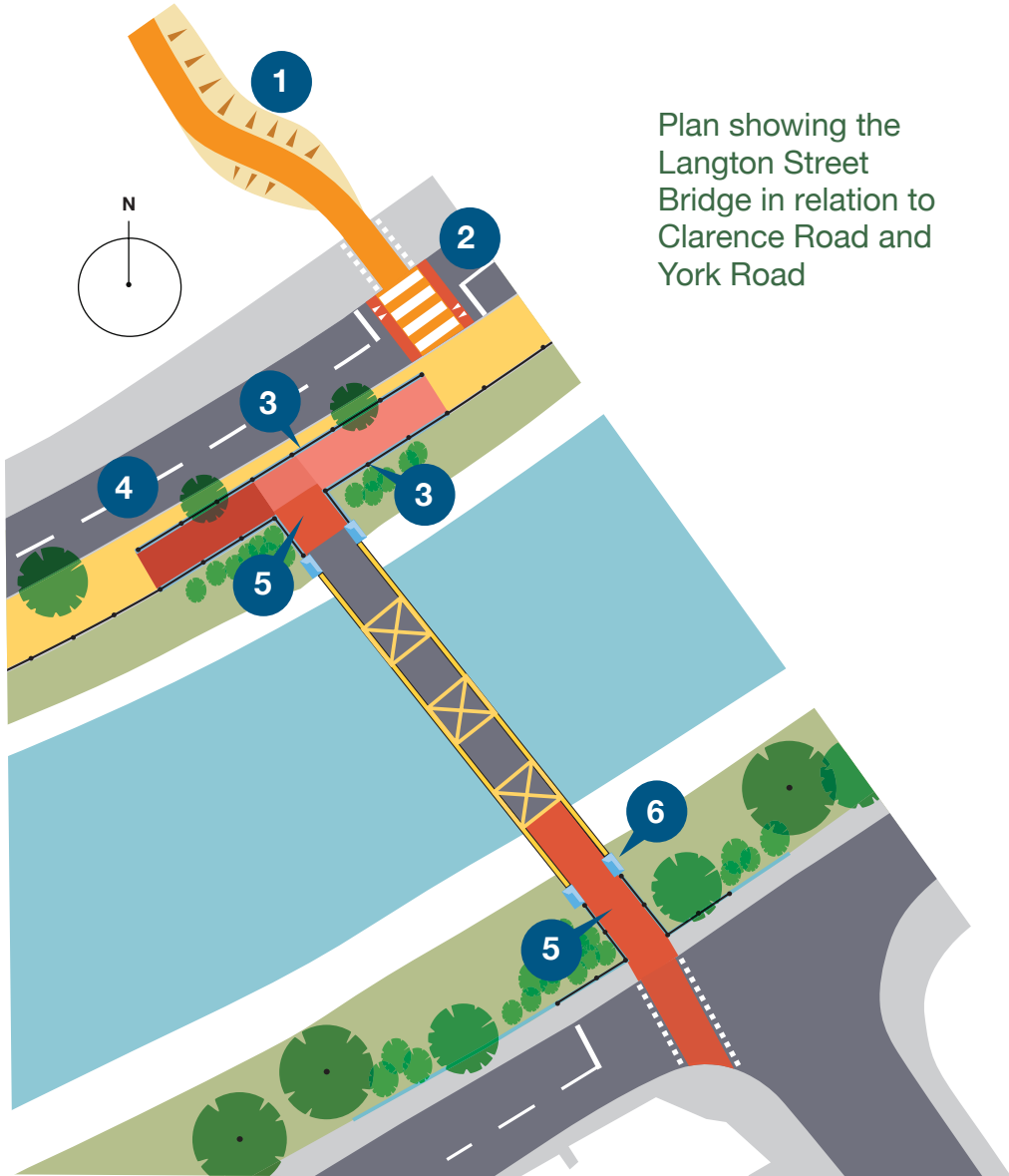


Bristol Promenades Project Langton Street Bridge Approaches

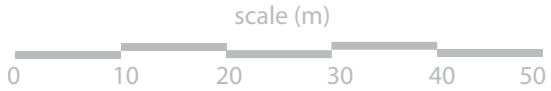
Langton Street Bridge is one of the original crossings of the Cut and the most direct route towards Redcliffe and the City Centre from St Luke's Road, Totterdown and Victoria Park. Either end of the bridge is approached by steps which these proposals seek to overcome with gently sloped ramps. One option might have been to raise the York Road end of the bridge and to lower the Clarence Road end. But this could prove difficult on account of services and the age of the structure. The alternative is to extend the length of ramp available by relocating the crossing some 20m to the east. This has the additional advantage of moving it away from the tight boundary. On the York Road side it is proposed to lift the existing flagstones over the last 8m of the bridge and set them to a higher level on rigid foam infill so as not to increase the loading on the bridge.

These works will provide a much more convenient route for pedestrians, one which is fully accessible to those in wheelchairs, and will allow the bridge to be incorporated into the City wide network of cycling routes.

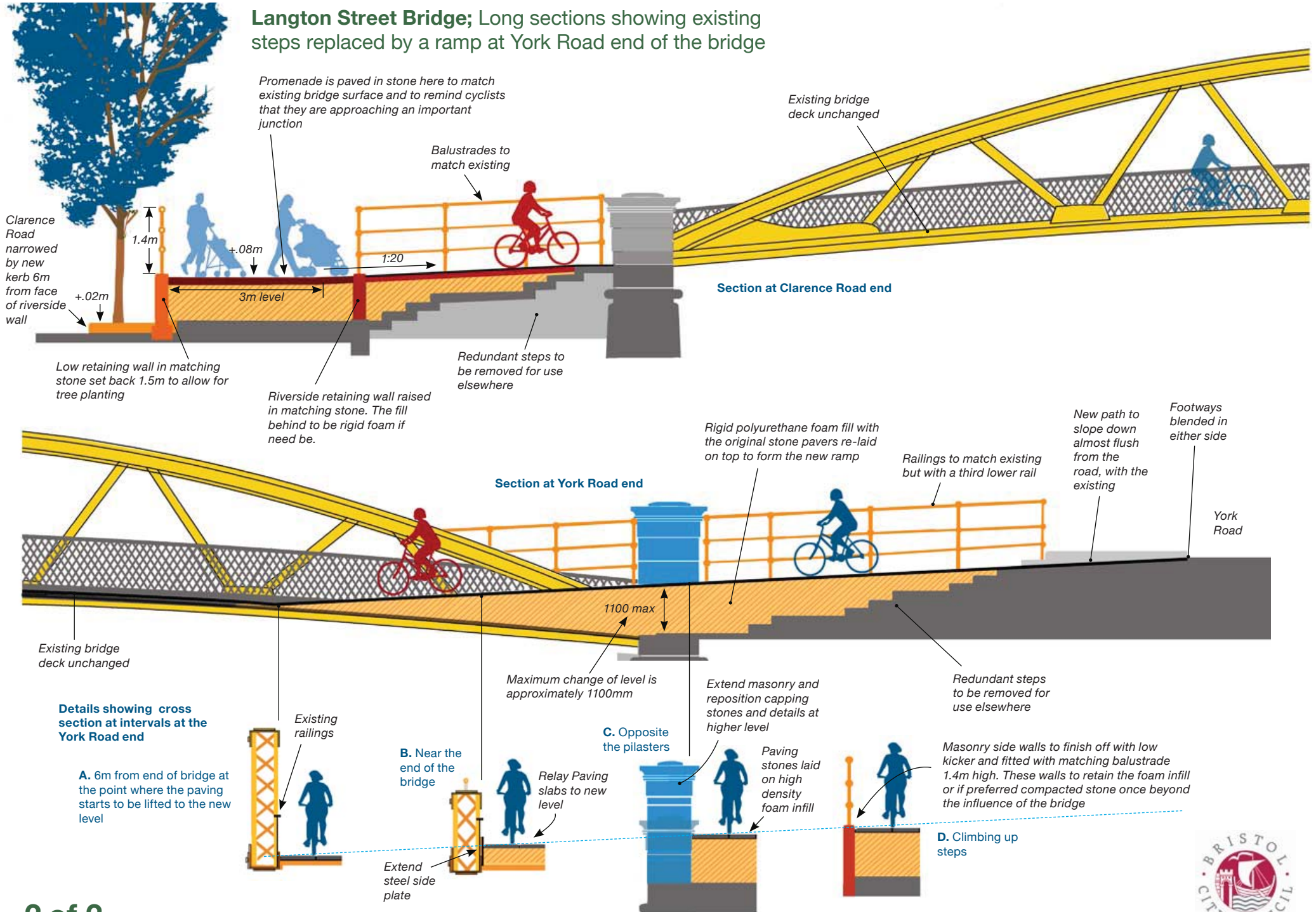
1. Reroute the path to line up with the proposed crossing and widen all through to a 3m wide path smoothly to Somerset Square
2. Existing crossing to be reinstated as a raised 'zebra' crossing 15-20m to the east.
3. Masonry walls to match existing constructed to secure fill and finished to 50mm above the new path level so as to act as a kicking strip. Back fill throughout with rigid foam if there is any doubt about the load bearing capacity of the ground here. Top off with iron balustrade to match existing.
4. Road narrowed to make 6m wide promenade. The low retaining wall to support the ramp rising up to the bridge is to be set back by 1.5m so as to allow tree planting through here. The promenade route climbs 600mm to the bridge landing and then drops away again.
5. Reuse existing paving to make new path at either end of the bridge running at a gradient of 1:20
6. Reconstruct the two pilasters at this end to match the raised height of the path.
7. Define York Road crossing with coloured surface and 'elephants footsteps' at existing light controlled junction and provide a clear phasing with short waiting times for pedestrians and cyclists.



Plan showing the Langton Street Bridge in relation to Clarence Road and York Road



Langton Street Bridge; Long sections showing existing steps replaced by a ramp at York Road end of the bridge



Bristol Promenades Project

Linking Langton Street Bridge over the Cut with Victoria Park and potential cycling routes to Hengrove – The Saint Luke’s Bridge

The Railway is a significant obstacle cycling south of the river. There is one almost traffic free bridge under the line at Windmill Close but it leads to steep hills and circuitous routes.

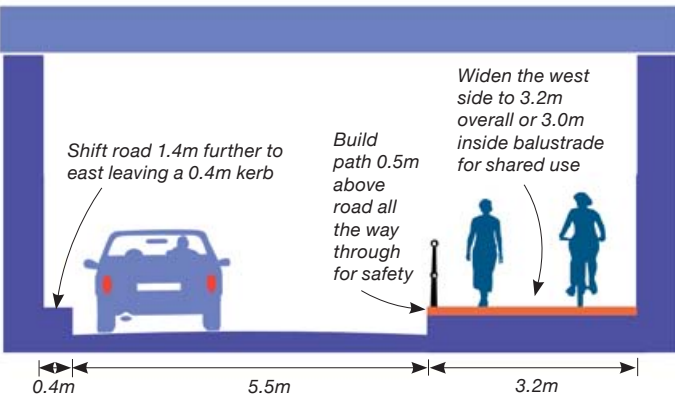
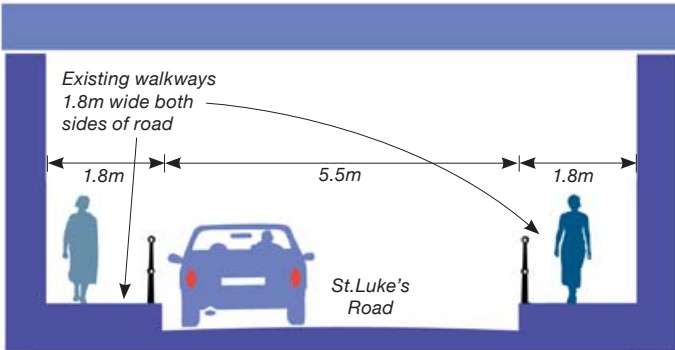
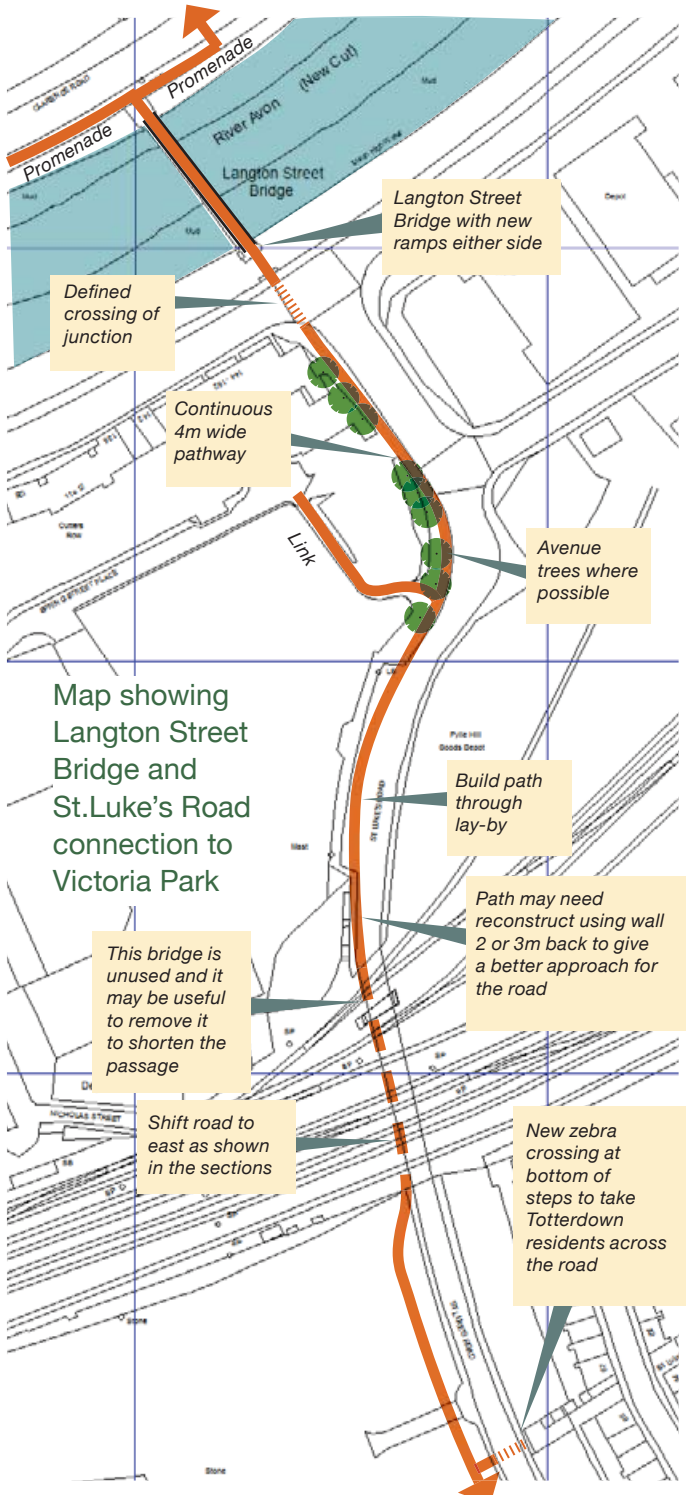
for those in Windmill Hill. It would also open up the potential of a Wedmore Vale route to the Hengrove area.

The Saint Luke’s Bridge is the direct route but its narrow carriageway with walks each side makes for a dangerous and threatening passage. Local cyclists often use the narrow footway which is dark and wet.

Saint Luke’s Road currently has two footways, 1.8m wide on each side of the road. If the road was reconstructed to the east then the west side footway, which is the most popular, could be correspondingly widened to achieve a clear 3.0m wide passage suitable for shared use.

A good route here would be of immense benefit. It would provide for an alternative to the Bath Road for Totterdown and Knowle residents, and a route

This sketch shows the arrangement and link from Langton Street Bridge to the Park.

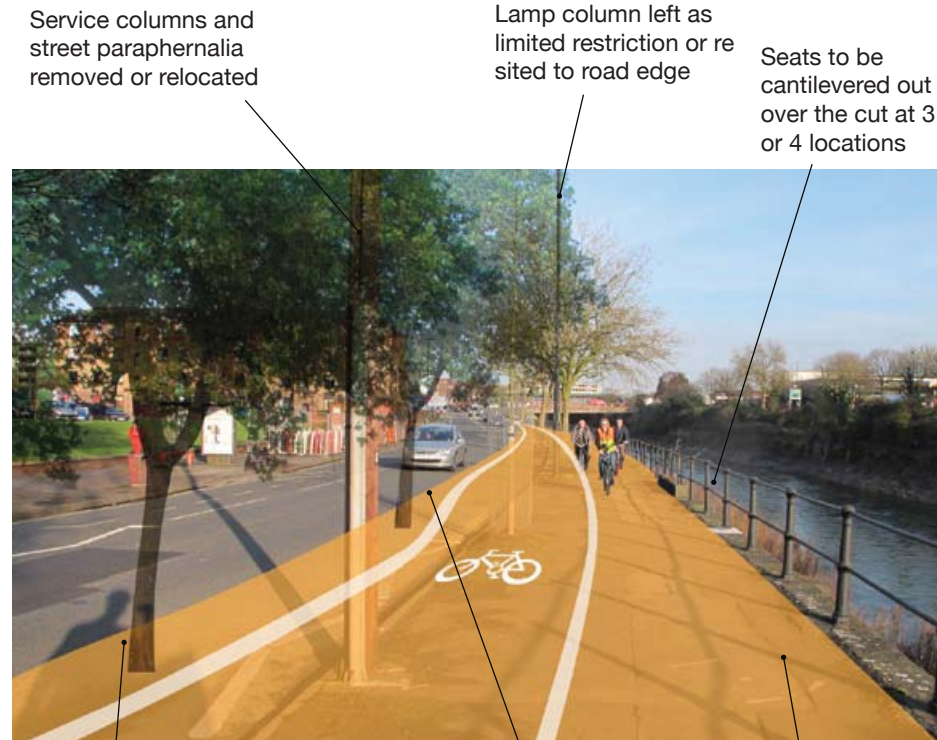




Bristol Promenades Project

Clarence Road looking towards Bath Road Bridge showing arrangement of 6m wide promenade throughout

Mark out cycle route 3.0m wide except where it can be reduced in width when passing trees. Maintain a clear 2.0m width for pedestrians against the riverside. Make sure that the path lining is set out in smooth organic sweeps rather than disjointed straight lines. The cycling route will run on the river side of the trees all through except when it has to pass behind remaining mature trees. As these are replaced over the years the path can be progressively moved towards the river.



Service columns and street paraphernalia removed or relocated

Lamp column left as limited restriction or resited to road edge

Seats to be cantilevered out over the cut at 3 or 4 locations

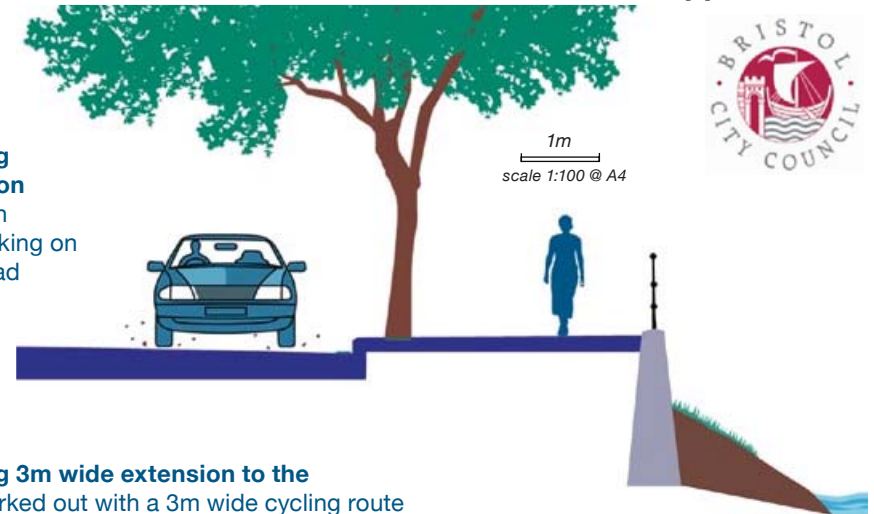
Recently planted trees to be replanted 0.5m from the new kerb line. Missing trees to be planted similarly.

Mature trees to remain as limited pinch points but to be replaced 0.5m from the kerb, either when they mature in years to come or when the newly planted trees either side have reached sufficient size

Parking along south side of road to be removed and existing kerb reset to approximately this line, 6.0m from the existing riverside balustrades

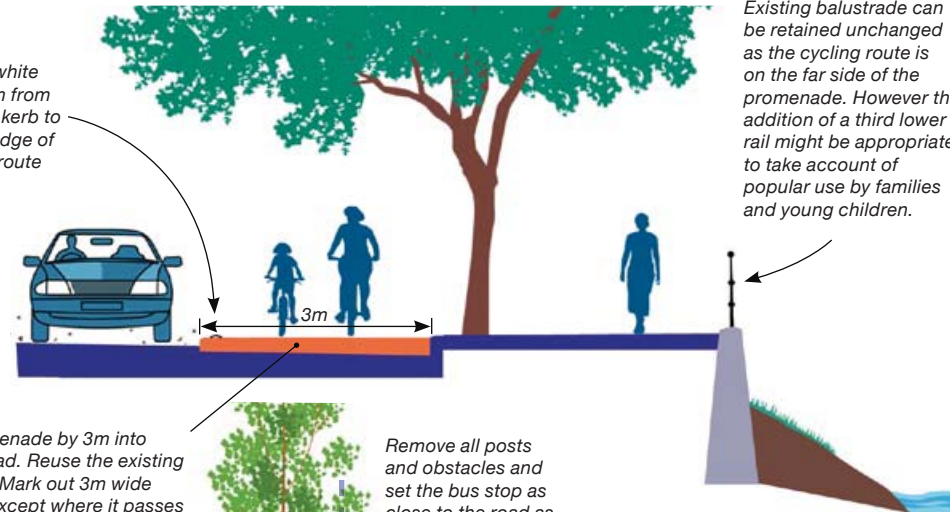
Resurface whole width of promenade

Sketch showing existing situation looking east with metered car parking on south side of road



Sketch showing 3m wide extension to the promenade marked out with a 3m wide cycling route

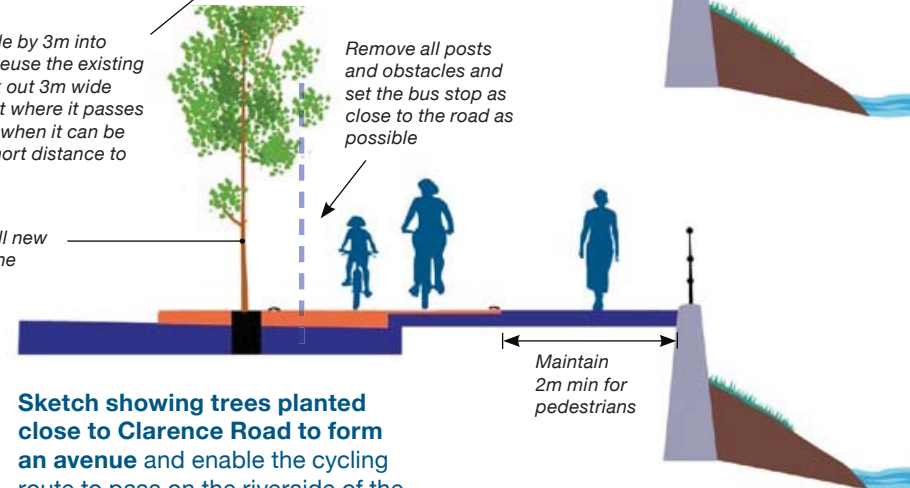
Mark solid white strip 150mm from edge of the kerb to define the edge of the cycling route



Extend promenade by 3m into Clarence Road. Reuse the existing kerb stones. Mark out 3m wide cycle route except where it passes established trees when it can be reduced over a short distance to 2.5m

Remove all posts and obstacles and set the bus stop as close to the road as possible

Replant all recent trees, and plant all new trees 1.0m from the kerb line



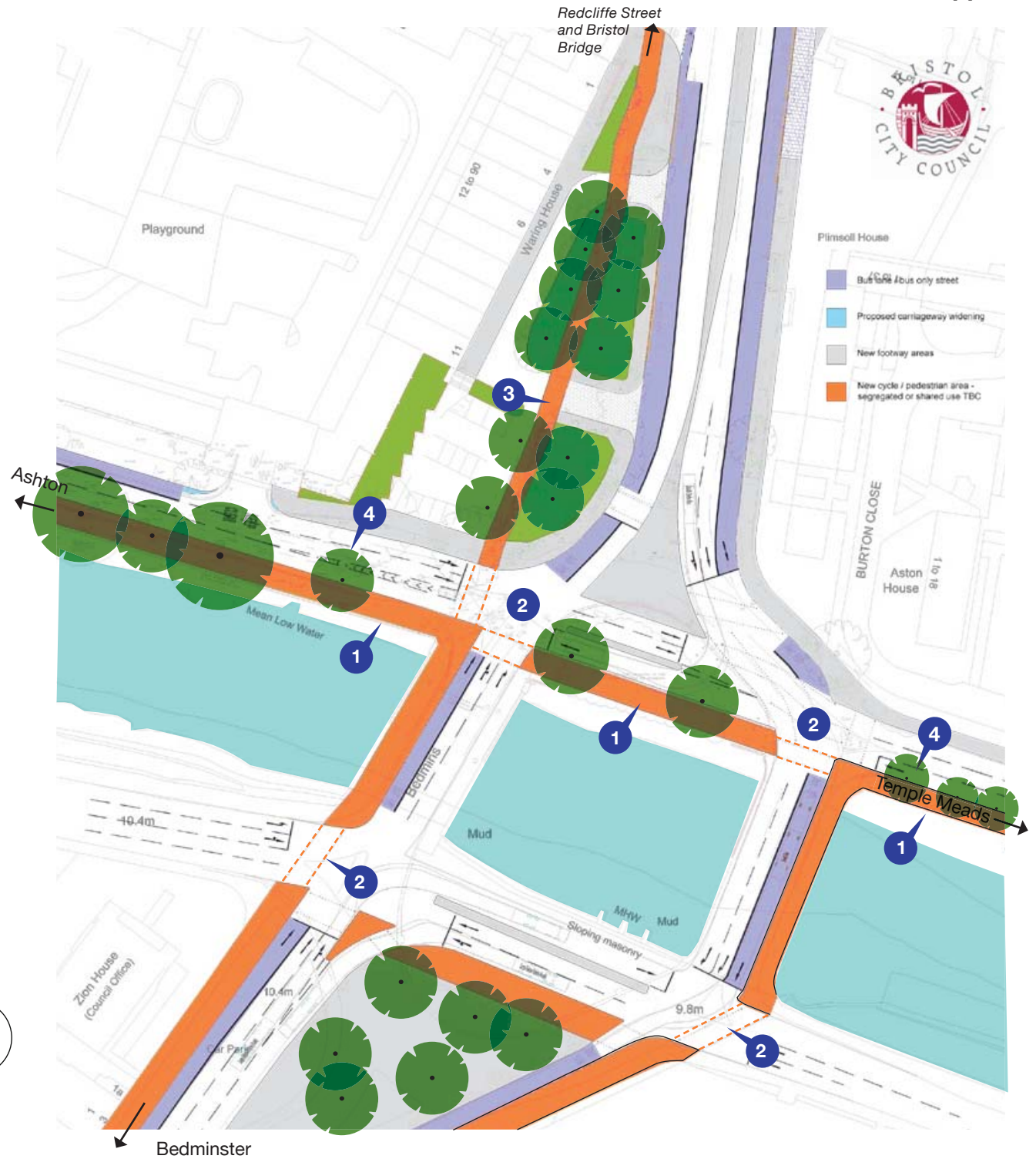
Sketch showing trees planted close to Clarence Road to form an avenue and enable the cycling route to pass on the riverside of the trees

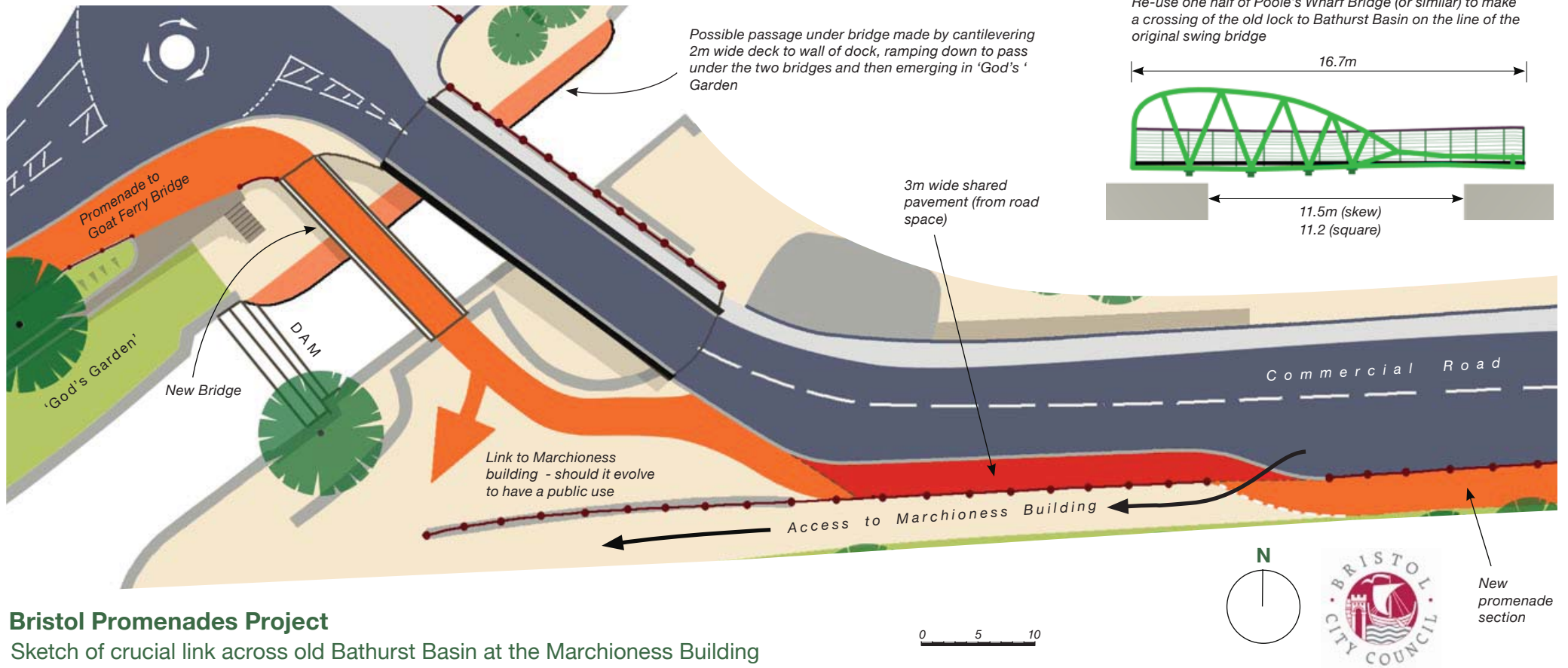
Bristol Promenades Project Bedminster Bridges

The Clarence road Promenade currently ends at the Bedminster Bridges gyratory. There is no direct route onwards along the Cut. What is needed is a single stage light controlled crossing to the central island e=which currently is a bit of an oasis. The sketch shown here simplifies the gyratory into a two strand cross roads which is the sort of arrangement which might be considered as part of highway remodelling.

As well as providing continuity of the Promenade, it is equally important to create a direct route from East Street up Redcliffe Hill to the City Centre. This should take the form of a tree lined promenade which could send us a memorable way to approach the glorious St.Mary Redcliffe Church for which a single stage zebra crossing should be positioned opposite the entrance to the Church Yard.

1. Widen the promenades to 6m overall throughout to allow for a line of avenue trees adjacent to the kerb and still leave a full 4m clear for walkers and cyclists.
2. Single stage pedestrian and cyclist crossings are required at all junctions. These should be as wide as possible, at least 4m, to match the effective paths.
3. This route up the hill should be treated as a "professional" way to St. Mary Redcliffe. It should be at least 4m wide, continuous across all roads and planted and a formal avenue up the hill.
4. Plant new avenue trees (London Place) adjacent to the new kerb line. Weave around existing mature trees but replace them nearer the new kerb line in due course.
5. Any road arrangements will end up with a lot of unused space all of which could be planted as a visual feature starting off East Street.





Bristol Promenades Project

Sketch of crucial link across old Bathurst Basin at the Marchioness Building

The riverside promenade does not exist over the short section between the wide pavement with mature plane trees opposite the old General Hospital site and the start of the Cumberland Road section to Gaol Ferry Bridge. At present one has to cross the road twice, possibly walking by Bathurst Basin itself to reach the Swing bridge walkway which lies on the east side of that bridge. This is not at all easy to do and the riverside route is effectively severed here.

What is needed is a new bridge over the old lock as shown in the sketch, or alternatively a raising of the height of the concrete dam to provide a more level route (and improve the flood defences of the City Docks) The sketch here shows the reuse of one half of the present Poole's Wharf Bridge should this become available following the proposal to reposition Brunel's Bridge there. The land in this area is privately owned by the Marchioness Building and this route would be subject to their agreement. At the same time the opportunity for public access to this attractive site, peering out over the Cut, would bring new possible uses there, as well as to 'God's Garden' which is currently almost unknown to the public. Access in this area would be further enhanced by adding a walkway through under the Commercial Road Swing bridge as shown in the illustration to link direct through to Bathurst Basin and the Ostrich .

Just to the east of this sketch there are proposals for a new BRT3 bridge over the Cut. Should these materialise then the details of this proposal may evolve, and a crossing of that road for pedestrians and cyclists will be crucial.



View looking towards Bathurst Basin showing possible relocation of Poole's Wharf Bridge and cantilevered route under the two bridges

Bristol Promenades East: The Feeder and the River

1. Create a 2 way cycle route along the north side of Feeder Road by extending the footway out to include the width of the existing cycle lane to give a 3m wide shared path (minimum).
2. Existing link to Barton Hill needs modification to its bollard at the end of the ramp.
3. A new bridge over the Feeder at this point would simplify crossing the water and avoid Barton Hill Bridge. Ideally this would line up with the existing level crossing.
4. Provide a zebra crossing over Marsh Lane to Netham Park.
5. Utilise existing zebra crossing link back to Kings Park Avenue.
6. Rearrange fencing to Foster's kerbline to create a good width for the path.
7. Raise path 300mm and realign under crown of railway arch. Maintain this level all though so as to be above usual highest tide levels.
8. Open up link to Showcase.
9. Reconstruct riverside path to a good standard in gravel to maintain a rural feeling on this remarkably backwater section.
10. Review the crossing of Whitby Road here which is rather awkward and continue the railway route through the Brethren site.
11. Complete this route with a link to the Paintworks, and a designed way over the A4 to reach Arnos Vale Cemetery for a route up the hill to Totterdown (Cemetery Road) which would be a most valuable way up this hill when the Cemetery is open.
12. The crossings of the two branches of Netham Road need to be defined and created. The eastern one at least requires a zebra crossing.
13. Make a link through to use the footway of New Brislington Bridge and connections to the existing riverside path.
14. Enhance this path where necessary.
15. The connection to the bridge could be improved as it is currently no more than a desire line. This is the last bridge over the river till the proposed bridge to Somerdale (excepting the High Level Ring Road).
16. Enhance this promenade and sign for shared use.

Bristol Promenades East: The Feeder and the River



Promenade Route ———— Linking routes ————

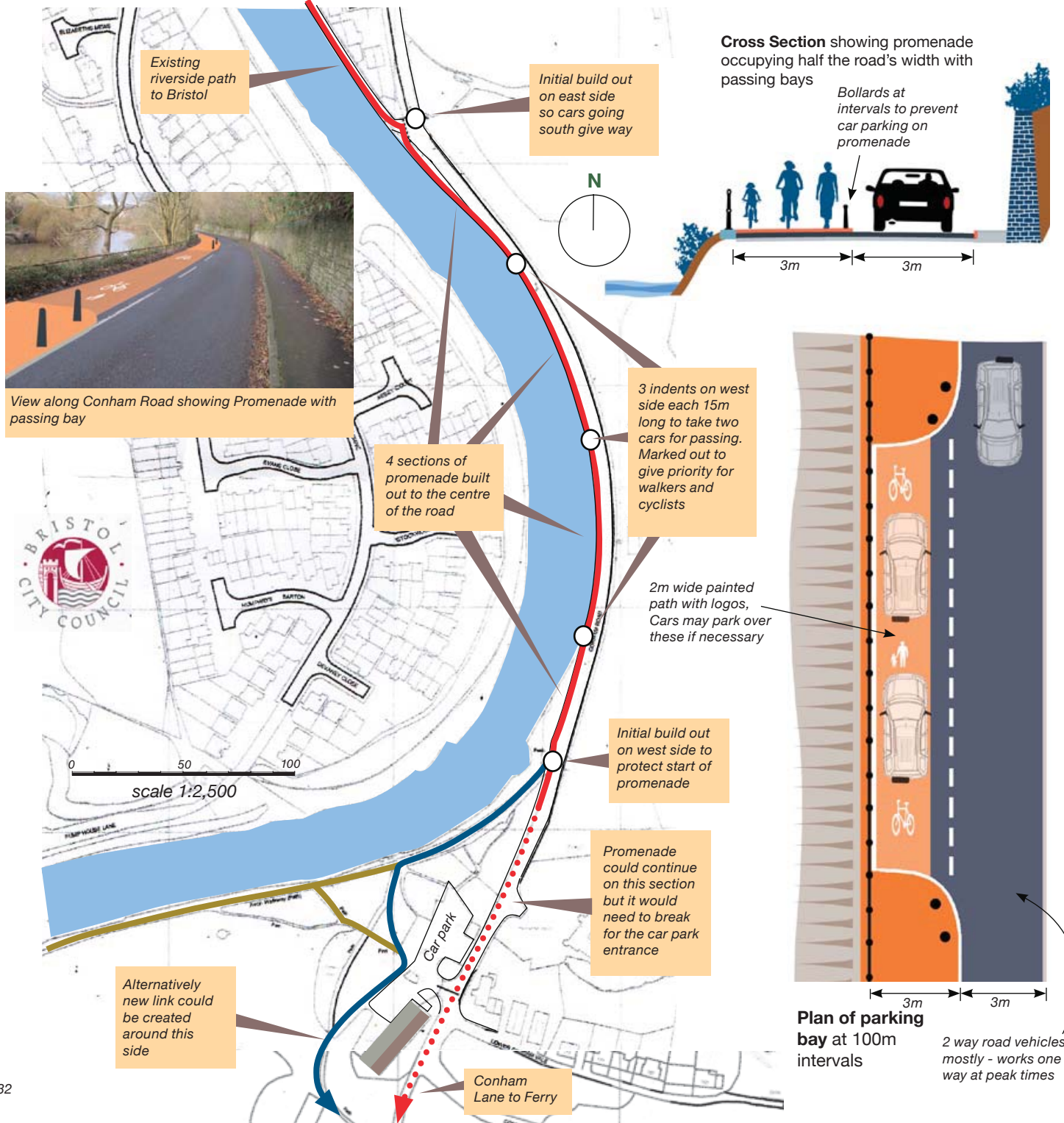
Bristol Promenades Project Conham Road Details showing proposal to carry promenade route past this long standing gap in the towpath route

The riverside path from Hanham comes to an abrupt end at the Conham Road. From this point there is no choice but that cyclists follow the road, whilst pedestrians must cross over to use the narrow footway crowded against a stone retaining wall all on the side of the road remote from the river. It is 320 long metres before the riverside path starts again running behind the new housing along Crews Hole Road.

The gap in the towpath has always been an unfortunate feature of the riverside route and one which has got worse and worse as traffic levels have increased. Numerous solutions have been looked at including bridging the river to pick up Wyatts View on the old paper mills side, cantilevering out to make a new walkway over the water, and various traffic management schemes. The best of these would be to close the road to through traffic as happened in 2010 when the retaining walls up Conham Hill were reconstructed. This felicitous state of affairs could be reinstated by closing the road at the bottom of the hill but leaving Conham Road itself open to local residents. Alternatively there have been proposals to introduce a one way system including Beaufort Road and Crew's Hole Road which would allow space for the promenade along Conham Road and also space for a cycleway on Beaufort Road.

This sketch shows the option of maintaining the two way traffic but only with a single width carriageway and the occasional passing space. This arrangement might be the most satisfactory because traffic on Conham Road is particularly tidal in nature and in the peak times travels mostly in one direction or the other. The drawing shows half the road on the riverside taken up by a promenade for pedestrians and cyclists with two or three passing spaces. The promenade would be marked out through these so that walkers and cyclists had priority, but the provided the space was clear cars could draw in to pass on another. These would generally be the few cars travelling against the rush hour flow.

At the southern end of the road, where it draws away from the river behind the car park, this arrangement could continue, or alternatively a new path could be built to the west of the car park as shown in the sketch. This might be a better solution as it would keep cyclists away from the sharp corner on the road at the bottom of Conham Hill.



Plan of parking bay at 100m intervals
2 way road vehicles mostly - works one way at peak times

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Bristol Riverside Promenades: Description of the Project, the purpose of this preliminary report and a Project Brief - Nov 2012

Introduction and background

The next phase of Cycling City's evolution will include a range of measures aiming to further popularise cycling and extend the proportion of all journeys travelled by cycle in Bristol. This whole programme might be said to have been launched in the '80's with the creation of the Bristol and Bath Railway Path, whose popularity demonstrated the opportunities for cycling as part of the transport strategy in the City, and has boosted ongoing programmes of cycling schemes since.

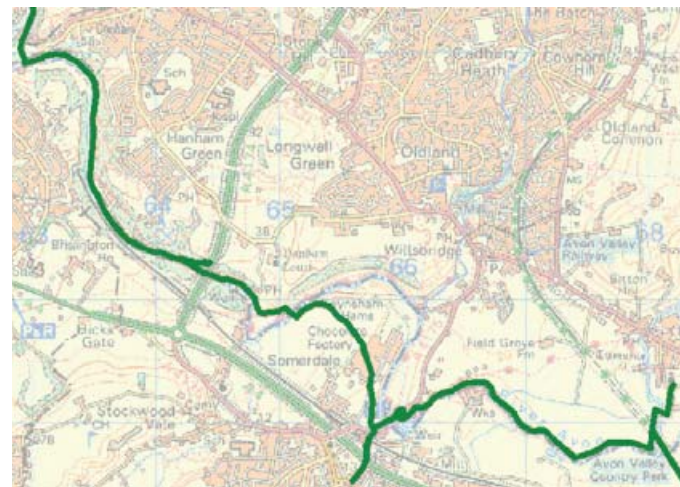
It would be valuable to create another world class "signature" route of this type in order to maintain this momentum and to further establish cycling as a popular and definite part of the way of Travelling in Bristol.

The corridor of the River Avon has long been the obvious opportunity for such a route. It runs through the whole width of the city, it's roads have wide pavements which could evolve into riverside promenades, and it could link the Avon Gorge under the Suspension Bridge with the Conham/Hanham Country Park which is another memorable part of the area's landscape.

Obstacles such as the Bedminster and Bath Bridges have always thwarted the realisation of this vision, but with the announcement of the Enterprise Zone we have a real opportunity of making such a classic route similar to the mainstay of many of Europe's Cycling Cities.



The redevelopment of the Somerdale Cadbury Site at Keynsham has been a useful catalyst. It provides an opportunity to create the long sought cycling route, from Keynsham to Bristol, by making a greenway through the Somerdale scheme and including a new bridge over the river to Hanham Mills and the start of the riverside path to Bristol. This work is the necessary first part of a new route to Bristol, and it could be extended through the whole width of the City to Ashton Gate.



Bath and North Somerset Council have joined together with South Gloucestershire Council to make this route happen. Their vision is for a good quality sealed path, similar to the Bristol and Bath railway path, all the way from Bitton to Keynsham, Hanham and Conham. They have appointed John Grimshaw to carry all the land negotiation and preliminary procedures, including planning applications where necessary, to deliver this route beyond the boundaries of the Somerdale site.

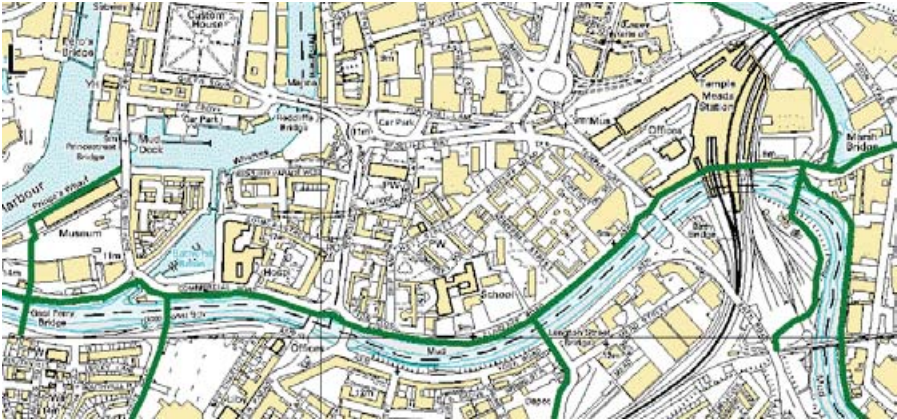
The existing towpath from Hanham to Conham is actually owned by Bristol as part of its Docks Estate. South Gloucestershire's boundary runs along the hedge line. For part of the way the towpath is in a very poor condition and despite repair works to the river bank two years ago some sections are very narrow. So the aim has been to acquire a narrow strip of additional land in order to construct a 3m wide path set back 2 or 3m from the edge of the bank top. This work has involved close liaison with a number of Bristol's officers including particularly Mary Knight who has long managed the right of way – now a bridle path. It is likely that the final path will straddle the boundary between the two authorities.

At Conham the route enters Bristol proper, and in discussion with John Richfield and Chris Orlik, it was decided to extend the 'Keynsham Greenway' to Temple Meads Station to give it a central destination and to ensure that the remaining problems of Conham Road (where there is no riverside path), Netham Road (where a crossing is needed), Marsh Lane Bridge and the Feeder Road Link, the poor riverside path from Kings Park Avenue under Brunel's Bridge and behind Showcase Cinema and, through the Post Office site to the Temple Quay area, were all addressed.

Now with the creation of the Enterprise Zone, and the next phase of developing the Cycling City in hand, there is the opportunity of extending this riverside route the whole way through Bristol to reach Ashton Gate, the riverside routes down river and the Festival Way to Ashton Park and westwards.

If a riverside greenway, promenade or cycle route could be made to a world class standard route with real continuity, attractive surroundings numerous connections and of course a level way connecting right across the City from North Somerset to Bath and Northeast Somerset and South Gloucestershire, it could have a huge influence.

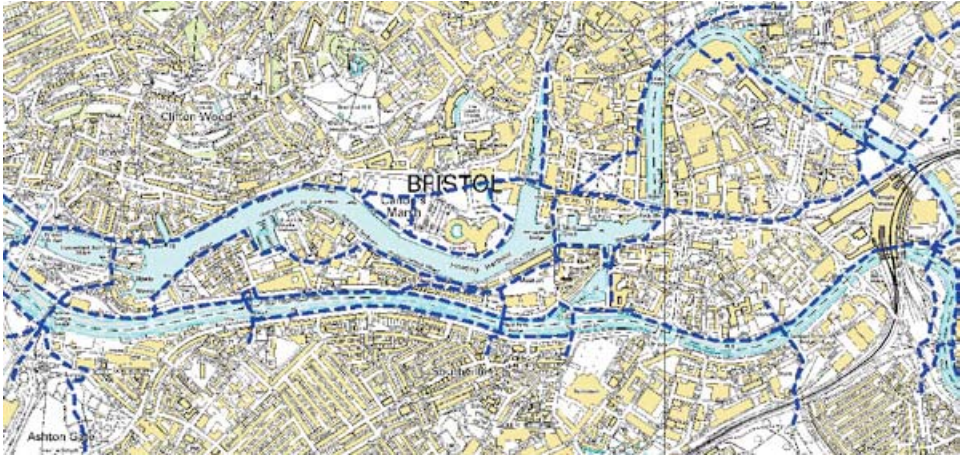
There are a number of problems to resolve including continuity and linkages at the new bridge planned for the Arena site, at the Bath Road and Bedminster Bridges, across the planned BRT3 Bridge and past the Bathurst Basin entrance. These current schemes all give wonderful opportunities to resolve longstanding blockages, and the adoption of this Avon Greenway route now would ensure that these chances are not lost to us all.



Extent of the Report

The Bristol Promenades cannot be solely confined to a simple east – west route along the Cut and the Feeder. For a start the original course of the river is now the City Docks, most of the quays of which are already extremely popular promenades and public spaces. So for completeness The Bristol Promenades Report should include all the sections of the waterways along which promenade and shared use routes exist or can be created. The report will consider what measures can be taken, if any, to further enhance these routes, and will specifically list those sections of waterside which should be maintained only for pedestrians, for example the west side of St. Augustine’s Reach where the press of people is too great and shared use would be inappropriate.

The report should also include comment on any immediate links required to give access to these promenades, including both what exists and what might be brought forward by developments or road schemes.



The characteristics of a Promenade

A promenade is rather more than just a traffic free route for walkers and cyclists to use. It is certainly this, but it also contains the idea of a certain pleasure, of leisureliness, of a meeting place for friends and acquaintances, of a focus for public life and of views over the countryside or city. Certainly the Bristol Promenades could achieve all this provided they can gain the space, the width, and the status to be seen as a good deal more than just another route.

For this reason this report will include numerous details to show how such an ambience could be achieved including particularly the essential need for real continuity of route through junction and across road all to such a standard of quality that the promenades get a small lift from the process rather than a grey feeling of subservience to traffic.

Bridges

It is the nature of rivers through urban areas that bridges are a real feature and those for pedestrians and cyclists only provide considerable benefits which encourage these modes of transport. So, for example, Pero’s Bridge and Gaol Ferry Bridge, are both central to the decision of many to walk or cycle at all.

The report will touch on new bridges of this type such as from Welsh Back as continuation of King Street, and across the Floating Harbour from near the SS. Great Britain. To be honest these are luxuries which it would be marvellous to see but might not be considered essential. However this is not the case at Commercial Road where

across the old Bathurst Basin Lock a new bridge is essential to complete a gap in an otherwise almost complete route along the Cut, or at Barton Hill Bridge which is maybe too narrow for an additional promenade route. Also the report will consider if Brunel's redundant bridge at Cumberland basin can perhaps be put in good use elsewhere rather than continuing its neglected retirement.

Discussions and Promotions

The development of the ideas associated with the Bristol Promenades could branch off in many directions and involve numerous people, so much so that the work could get out of hand. So I will use my discretion to do what is necessary to best promote the project and to tease out its most interesting possibilities.

In the first instance there will be regular meetings with John Richfield, the Client, to cover matters as they arise, and secondly all discussions with third parties will be recorded, at least in general, so that the City can see how matters are progressing.

The Presentation of the Report

The purpose of this report is to convince the policy makers and decision takers in the City to adopt and wholeheartedly promote the Bristol Promenades. The report will present the key details – passing under Temple Meads on a floating promenade, widening the footway of the Feeder, anticipating avenue trees on Clarence Road and so forth, as a series of key illustrations.

Once these are adopted then the rest of the routes can be joined up in a fairly straight forward way. The overall promenades will be detailed against a map base of a 1:1250 scale annotated with notes and pictures.

The Report will be presented on an A3 landscape document so that it can readily be distributed to those who do not have access to electronic displays.

Once John Richfield is satisfied with the work, I could make a presentation to the public or Council as it seems best.

Timing and Programme

A work of this kind has no beginning and no ending! Some of the promenades exist, and it is certain that even if they follow the general scheme set out in this report, they will evolve, hopefully always for the better, as time goes on, developments adopt the proposals or new transport and other schemes come forward. So it is unlikely that the Report will ever be finalised but might better be seen as an evolving document which other may wish to take forward.

But as a first target, a draft for public use will be ready by Easter 2013, although details can be brought forward as the need exists.

Estimate of Cost

Promenades can be notoriously difficult to cost simply because their standards can vary so enormously, and because some of their best parts may come about by inclusion on other schemes, for example, the new bridge link to the Arena Site.

However despite this, the Report will provide a framework for estimating the cost of the Bristol Promenades.

John Grimshaw

November2012

